

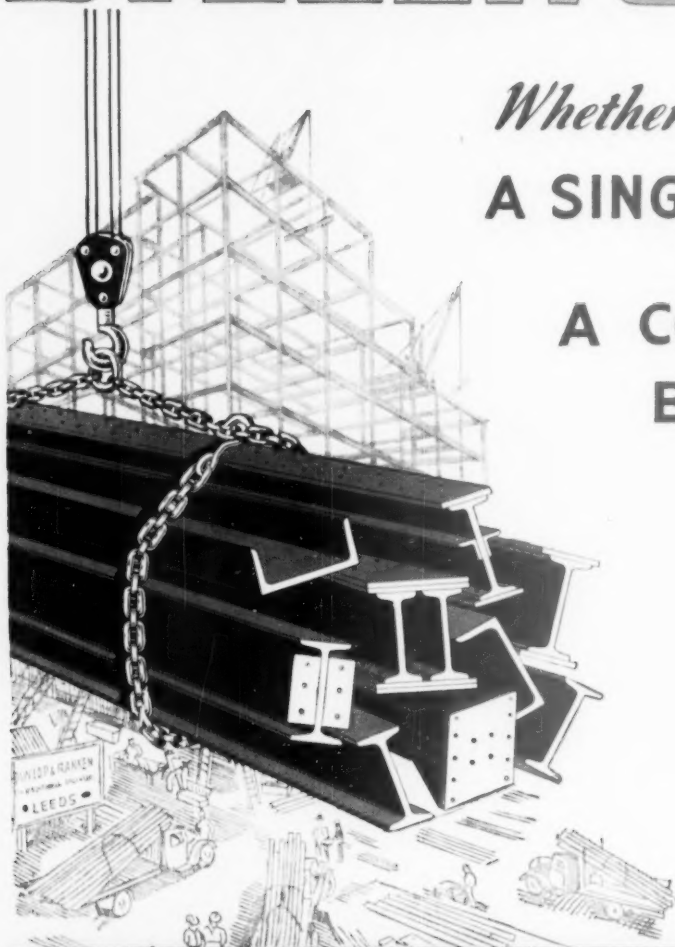
THE  
ARCHITECT  
& BUILDING NEWS

*In this Issue*

- DAIRY FARM, GREAT CRANFIELD, ESSEX
- WAREHOUSE AT HENLEY-ON-THAMES
- SINGLE SPAN HOUSE

AUGUST 18, 1950 • VOL 198 • NO 4261 • ONE SHILLING WEEKLY

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It was the age of Drake and Raleigh, of Shakespeare and Sir Philip Sidney, when discovery and adventure were the rule in art and life. Poets, playwrights and pamphleteers delighted in letting their new-made language run riot. In their exuberantly decorated houses—carved wood and moulded plaster, stained glass and silver plate—the extravagant costumes of the nobility

were quite in place. The Renaissance had brought in different standards of behaviour and new expectations of comfort. The Elizabethan builder was more than equal to his task of adapting old designs to meet these changes in society. We of Celotex believe that in this present century we are playing an equally important part in using new materials for fresh needs.

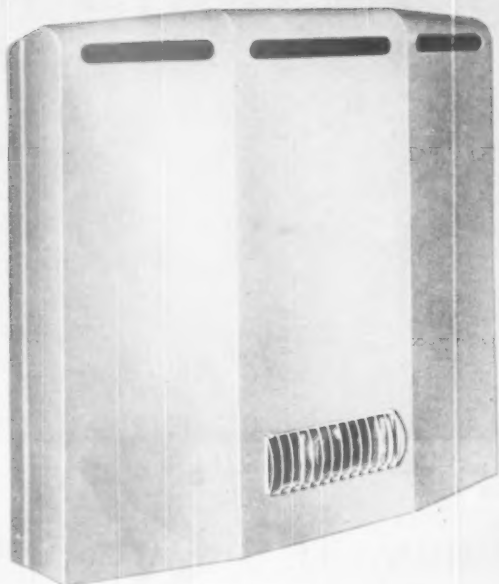
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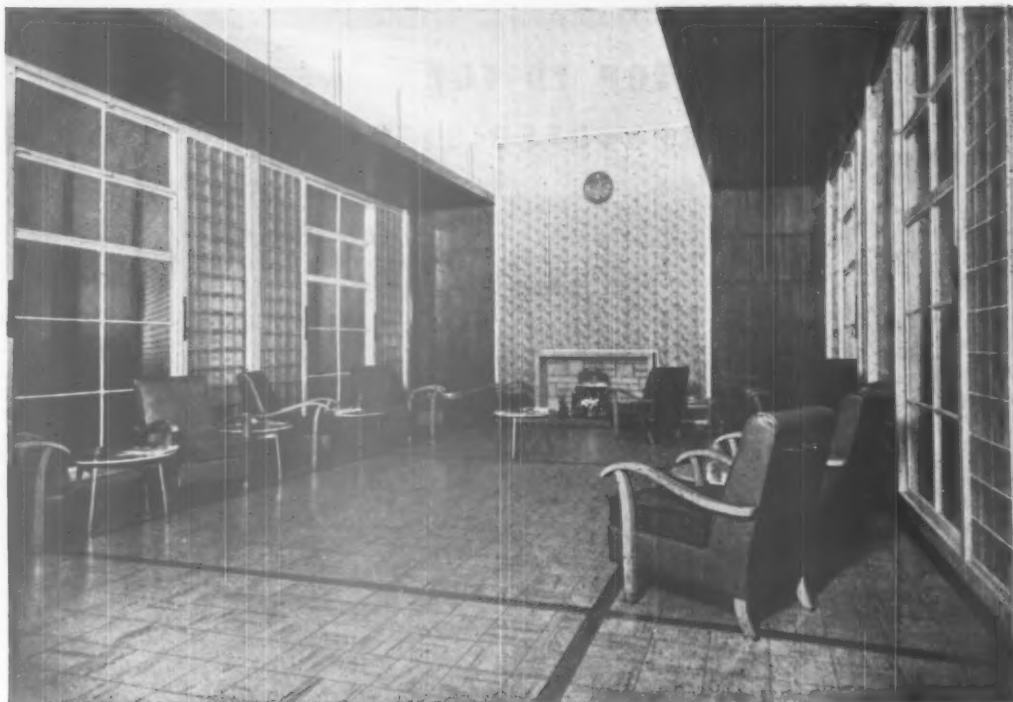
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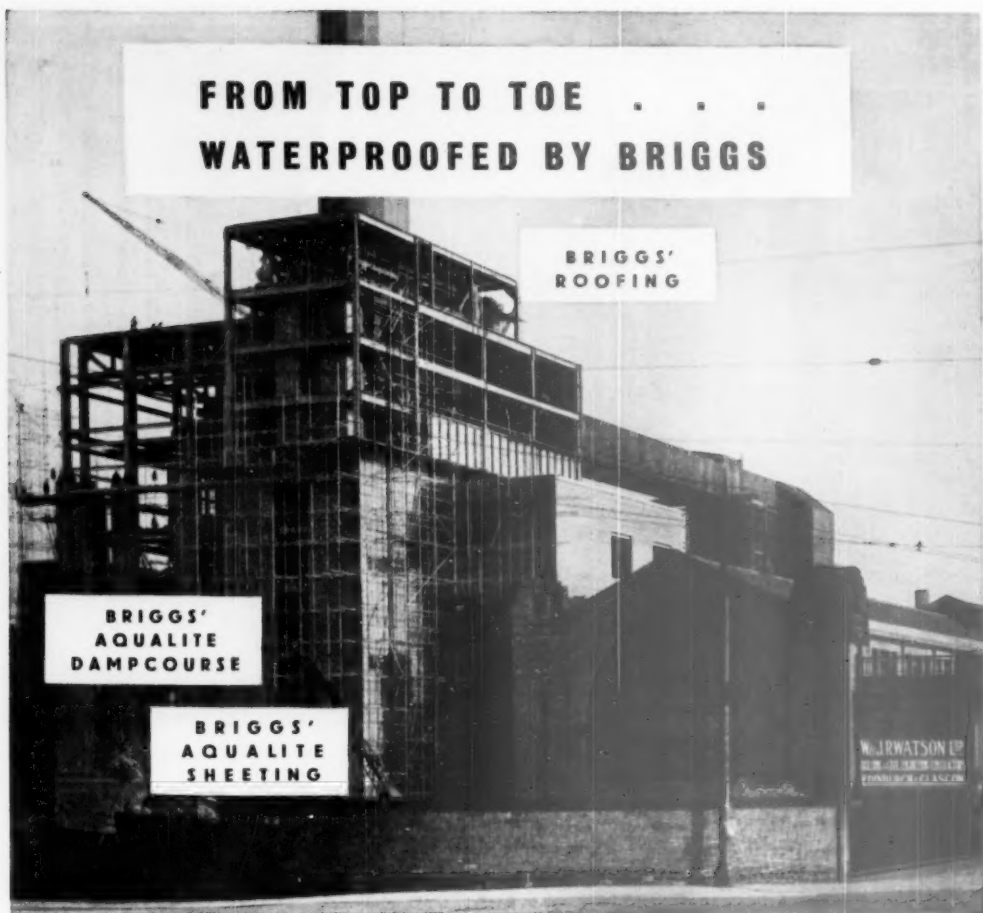
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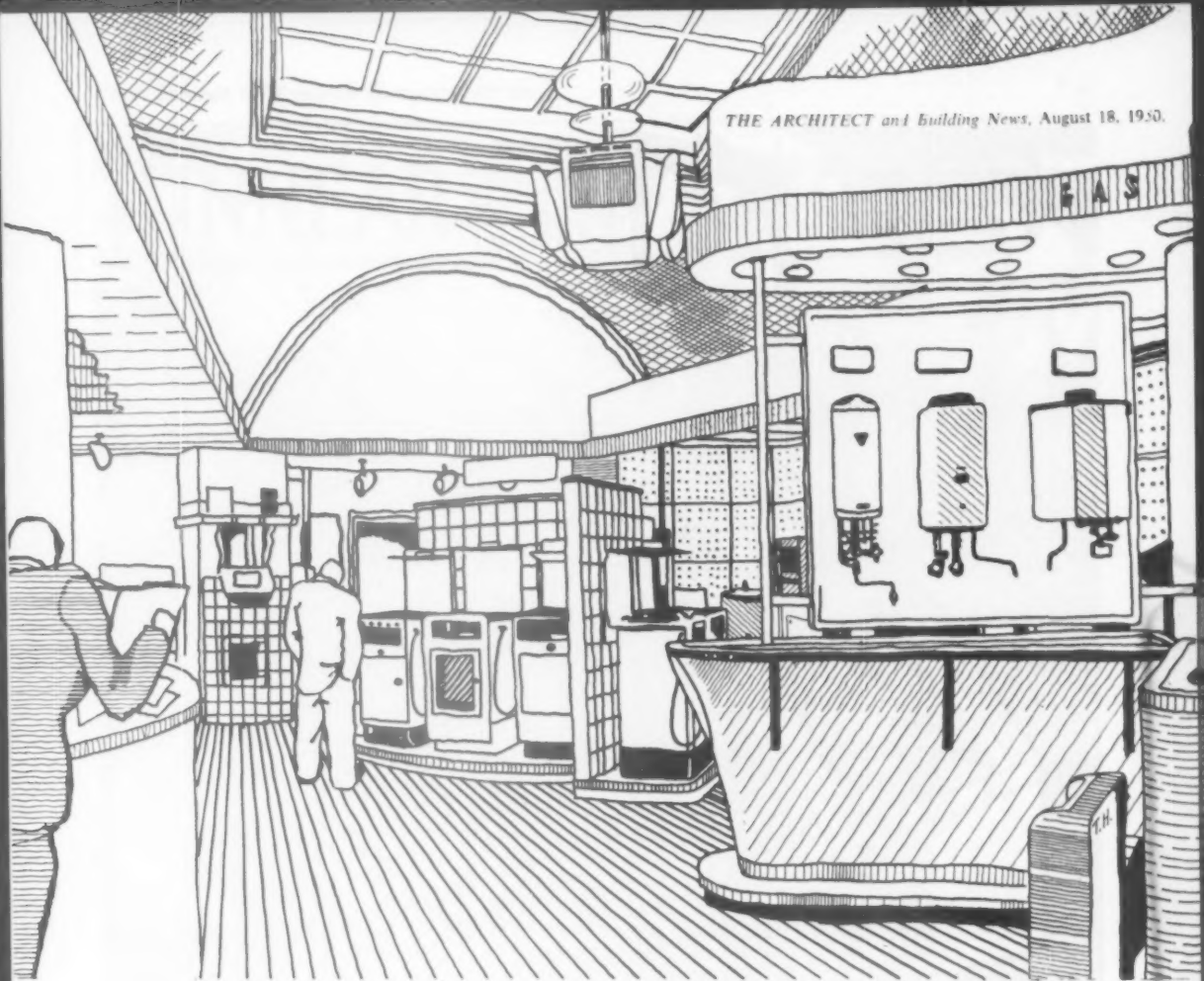
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## “GAS in the design for living”

UNDER THIS TITLE The Gas Council maintains, at the London Building Centre, a permanent exhibit the object of which is to provide a convenient centre to which architects, builders and housing authorities may come in order to keep abreast of the latest developments in gas services. The exhibit is largely devoted to the domestic uses of gas and coke, but also deals with large-scale catering equipment and other commercial and industrial appliances. A technical assistant is in attendance to give information and advice, and visits from individuals or organised parties will be welcomed (the latter preferably with prior notice).


*This exhibit is one of many activities by which The Gas Council seeks to promote the efficiency of gas services for cooking, hot water, space heating and refrigeration. In all cases where the use of gas is envisaged early consultation with the local Gas Undertaking is advisable.*

# GAS



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## Here are some important facts on draught exclusion

- **THE AVERAGE GAP** around windows and doors through which infiltration takes place, i.e. the gap between frame and sash, or frame and door, has been shown as a result of many thousands of tests, to be about  $\frac{1}{4}$  of an inch.
- **BEFORE DRAUGHT-PROOFING.** Experiments on double-hung sash windows with such an average gap, and under a pressure produced by a typical wind speed of 10 m.p.h. revealed an infiltration rate of 186.9 cu. ft. per hour per foot of gap. Such windows, 5' 0" high and 2' 9" wide, and having therefore a gap LENGTH of about 18' 0" showed a total infiltration rate per window of 1908.9 cu. ft. per hour.
- **AFTER DRAUGHT-PROOFING.** The infiltration rate per foot of gap was reduced to 14.7 (as against 186.9 cu. ft. per hour, and for the whole window was reduced to 264.6 (as against 1908.0) cu. ft. per hour.
- **PREVENTION OF INFILTRATION** was therefore 1643.4 cu. ft. per hour per window, or 86.1%. Tests on casement windows have shown results up to 95%.

If the magnitude of the cold air infiltration rate is startling, the percentage of the reductions achieved by draught-proofing is equally so. What are the overall results?

- ★ About 50% of all the heat lost through the average window can be saved.
- ★ About 20% of all the heat lost in the average house can be saved, assuming a comprehensive installation.

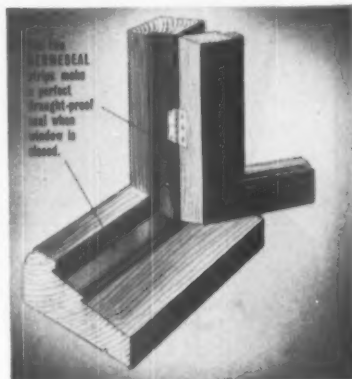
### IN ADDITION . . .

- ★ Draughts are banished.
- ★ Fuel is saved, and normal warmth and comfort can be enjoyed in cold weather when heat output would otherwise be inadequate.
- ★ Heat is conserved and internal temperatures raised in proportion to the heat loss prevented.
- ★ Entry of airborne soot and dust is reduced, with consequent saving in domestic work, particularly in cities and industrial areas.
- ★ Noise is minimised from external sources, and rattling windows and doors prevented.
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**THE COST** is calculated on the number of feet of Hermesal installed, plus that of any drop-seals or thresholds fitted, to which is added the cost of fitters' fares and/or subsistence as from the Company's works at Croydon, or from Branches. Any carpentry work that may be found necessary to render woodwork fit to receive an installation, or sealing work for cracks, skirting boards, etc., is charged for extra on an hourly basis, plus the cost of any materials used.



# HERMESEAL

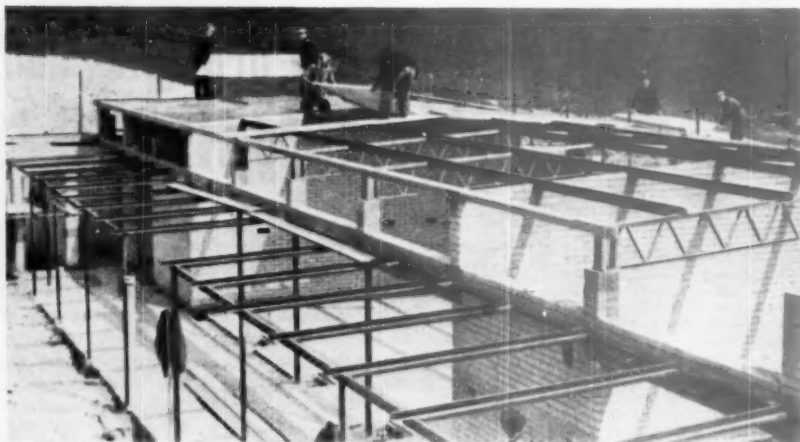
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## STRAMIT DECKING



This photograph shows STRAMIT roofing slabs (4' 0" x 8' 3") being laid to form a decking for bituminous felt at Elaine Avenue Primary School, Strood, Kent. (Architects: Messrs. Moiret & Wood, A.R.I.B.A., London in collaboration with S. H. Loweth, Esq., F.S.A., F.R.I.B.A., County Architect. Roofing Contractors: Neuchatel Asphalte Co., Ltd., London. General Contractors: Kent & Sussex Contractors, Ltd., Sidcup, Kent. Special purlins to the registered design of Messrs. Moiret & Wood).

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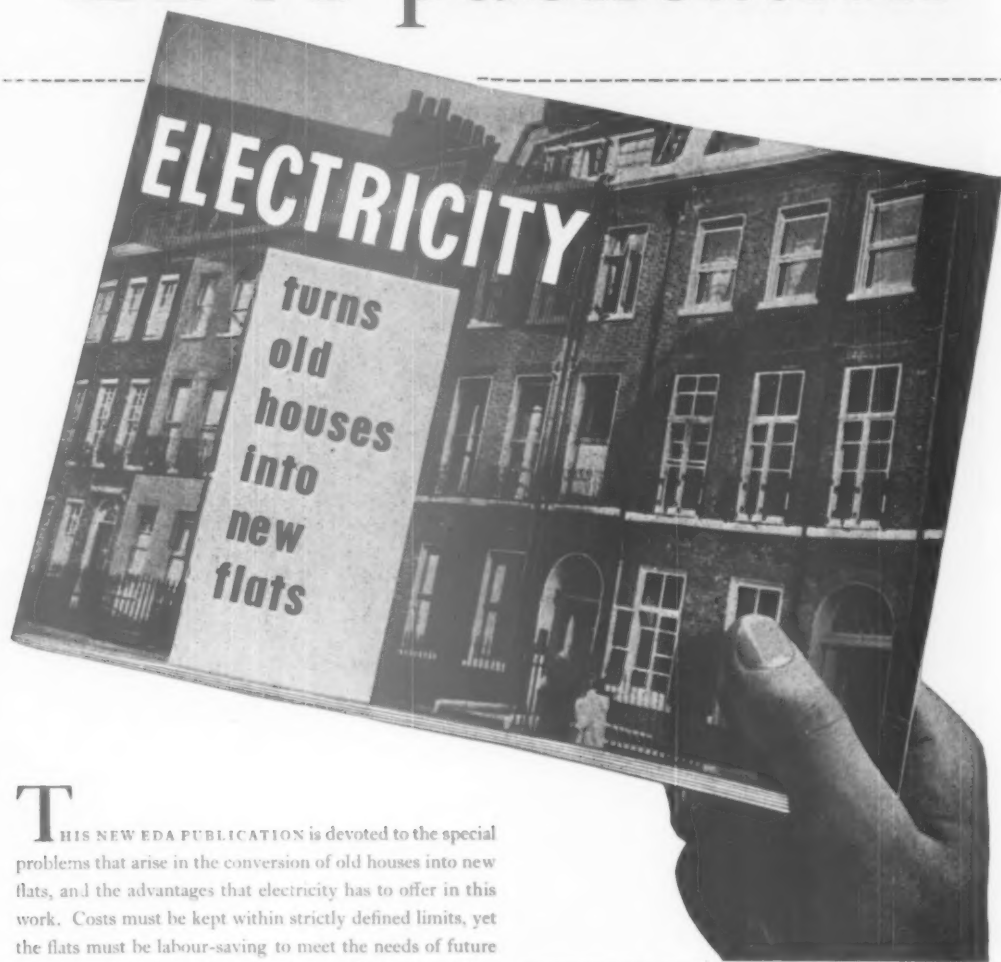
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*Architects and builders are invited to write for copies*

THE BRITISH ELECTRICAL DEVELOPMENT ASSOCIATION, 2 SAVOY HILL, LONDON, W.C.2



*This view of the Mechanical Test Laboratory at Reliance Works, Chester was drawn by Will Nickless for Williams and Williams Ltd.*

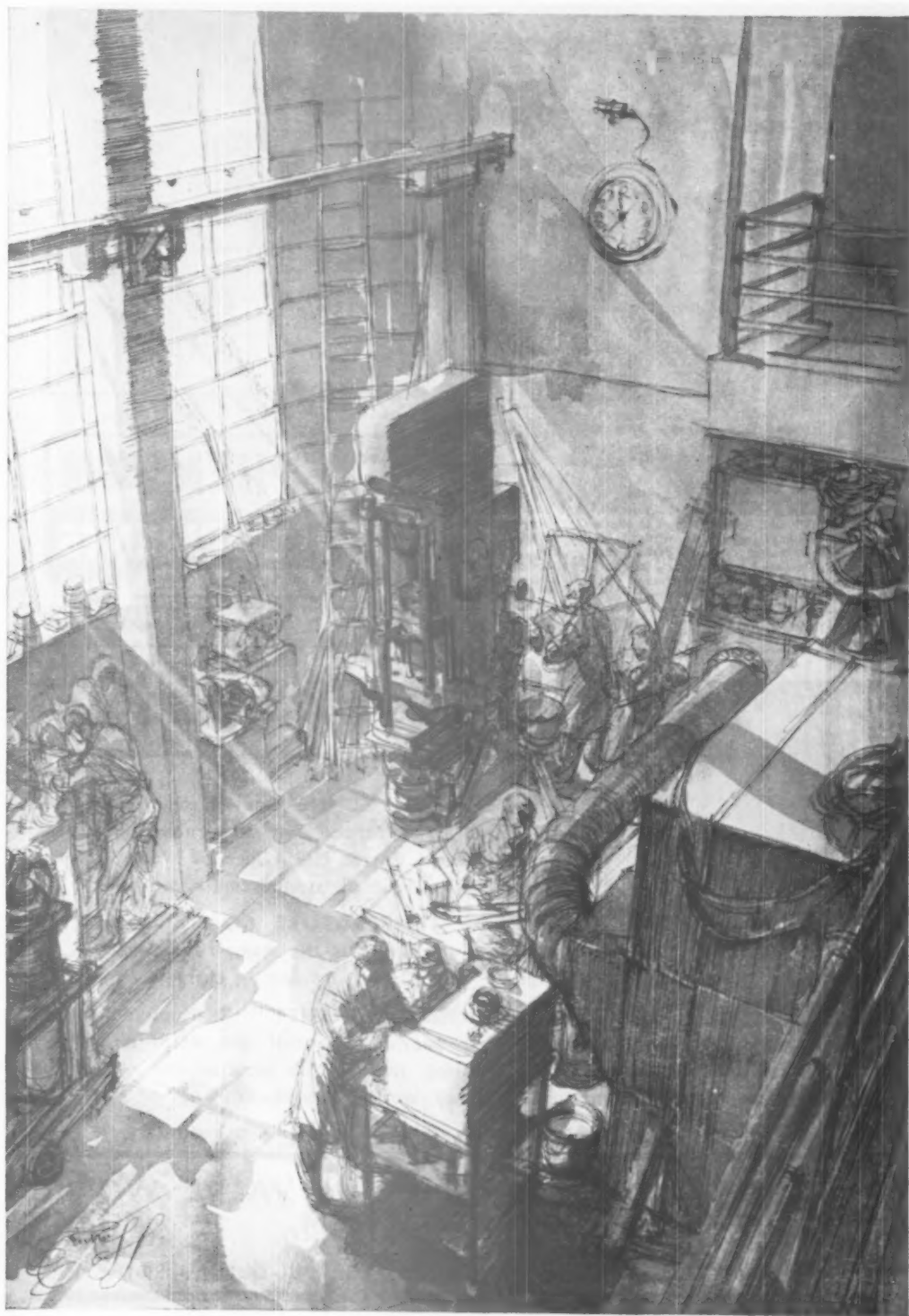
Do you ask awkward questions about metal windows before you specify them? "Have these sections been tested?" or "How was the breaking strain of your frame-welding tested?" If you do ask such pertinent questions, Williams and Williams can answer them. For the staff of the Mechanical Test Laboratory at the Reliance Works, Chester, are there to guard both the client's interest and the firm's reputation in precisely such matters. They are well equipped and possess, as the artist's drawing shows, a machine (right) for carrying out tests under wind pressure. Architects who specify Williams and Williams windows can rely on the scientific integrity of the men in the Mechanical Test Laboratory.

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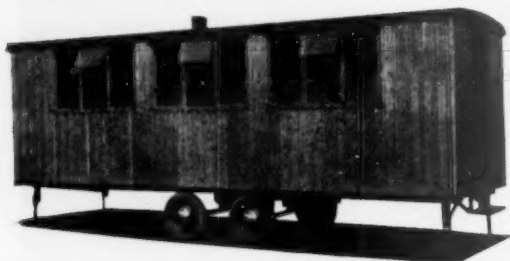
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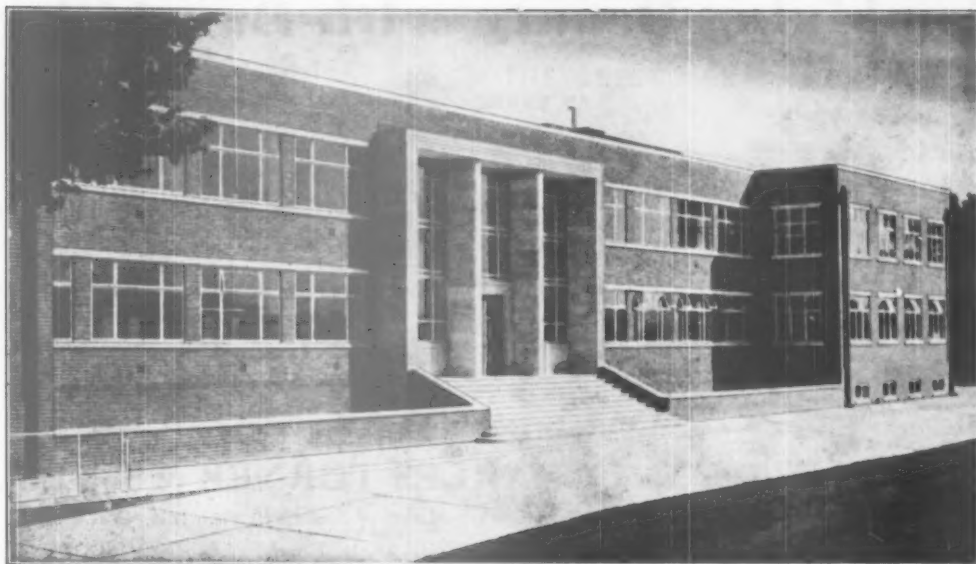
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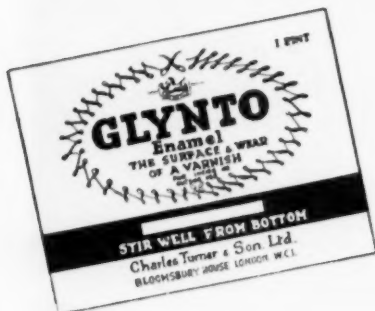
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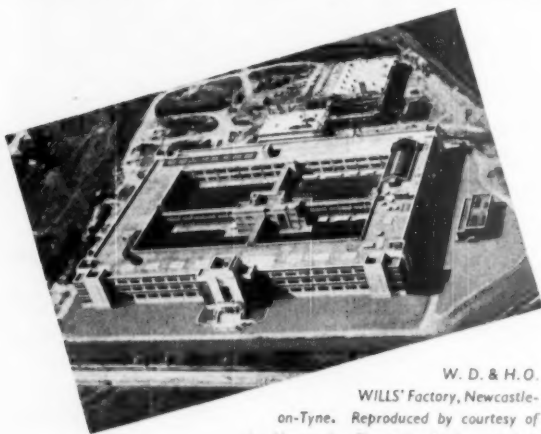
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## J A M T O M O R R O W

**I**N spite of the permanent improvements and temporary expedients already suggested by the police and planning authorities, the Commissioner of Metropolitan Police still says the handling of the extra traffic likely to arise from the Exhibition of 1951 will remain an enormous task and that "it requires no stretch of imagination to foresee the most serious difficulties in coping with the traffic both inside and outside the Festival grounds".

It is not only the size of the probable mass of pedestrian and vehicular concentrations within the South Bank and Battersea areas that is likely to prove difficult but the fact also that all through the 1951 period the normal services and communications have to be maintained as well. The fact that the areas concerned are within natural concentrations of cross-river bridgeheads does not contribute to the easing of the problem. And, so far, very little has been suggested that seems to make any major contribution to add to the piecemeal new islands and roundabouts and the adjustments to Parliament Square now being undertaken. Is there a solution?

The first and most obvious one is to keep the exhibition traffic away from the actual area, make it stop well away and see that all visitors enter the festival grounds on foot, by underground or from through-buses and coaches. A hard solution and

perhaps not a desirable one. Something similar to this idea would be the establishment of an adequate "ring route" around and by-passing the Exhibition, to be used by through traffic. But does such a route exist? It is too late to construct one.

The preparations made for the New York World's Fair in 1939 were much greater than anything so far contemplated in London and congestion was there largely avoided. A little more comparison of notes might not be amiss.

It is possible, we suppose, that some statistician will, after the event, work out and add to the cost of erection of the Festival of Britain the cost to the country of the consequential traffic delays in London for the four months or so of the Exhibition. We wonder if there will be very much difference in the two figures?

But what of the permanent problems in London? There is, over the whole country, now one car for every nine of the population old enough to drive and the proportion is likely to be higher in and around London. Most private cars move about London half-full with an average of, say, 1½ persons per vehicle; buses and taxis are also seldom full, except, perhaps, for the brief peak-periods of each day. New York partially meets this anomaly by keeping its private traffic out of the centre and sees that journeys are

completed on train or bus. Although this is probably not a good solution for London, some modification of it may yet *have* to be applied to the traffic of our own capital city.

Ultimately, if road traffic is to be allowed into the centre of the City and the West End, the twin problems of alternative routing for through traffic and the parking of cars and commercial vehicles will have to be met quickly.

Ring routes and radial routes (with no side entries or direct access to them inside the central area) for through traffic will ease the circulation of slower and more local traffic, but will often need extensive new constructions. Local traffic is, however, bound to be congested, slowed down and even stopped by a lack of parking facilities within the areas of urban concentration and high buildings.

Last year still one more scheme was submitted for the construction of a car-park under a London Square. This is an idea that comes to the front with the utmost regularity, but, so far, none have been built. Is the cost too great? Are the Squares too small? Why not try one out somewhere? If there is a hole to be dug, why not a big hole? Make it an underground multi-storied car park; it might be useful, also, as an air-raid shelter when cars go off the road again. In any case it is practically the only place in which such a thing could be done, for the land under the London Squares would be found to be reasonably free of the tangle of sewers, mains, cables

and tubes that runs beneath our streets and many of our buildings.

One result of underground car parks in London Squares would be hailed as an architectural improvement on the one hand and with howls of execration from the "lovers of the trees" on the other. The present aspect of the Squares, with their overgrown, woodland trees would have to give way to something like the openness and garden-like planting of the 18th century—which, judging from contemporary records, was presumably the sort of design that was originally intended.

Parking on bombed sites cannot be counted upon to help us for ever and multi-storied garages built above ground are probably uneconomic within the central area; therefore, before any more of the Squares are expensively rehabilitated after their wartime use, would it not be as well to get this recurrent suggestion tried out or put aside finally as useless? Two important Squares at least have been almost entirely reconstructed since the War—Grosvenor Square and Parliament Square; in neither place has the parking problem been considered adequately. In the case of the former the cleaned-up appearance can only be appreciated—on ground level—from the inside of the open space, never can it be enjoyed through the serried lines of parked super-streamlines that haunt its curb-lines and encumber the traffic-ways.

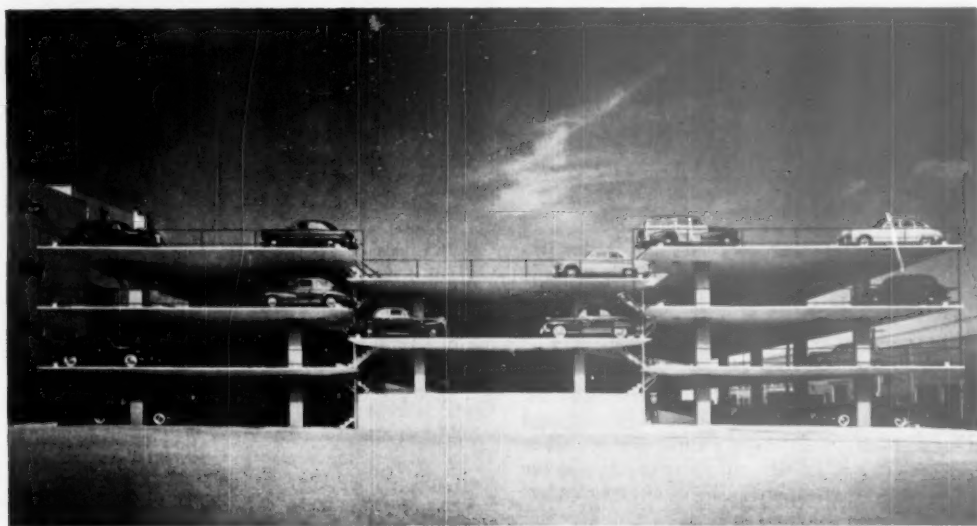


Photo: Joseph B. Brignolo.

## C A R P A R K F O R A F I N E C L I M A T E

A multi-storey garage in Miami, architect: Robert Law Weed and Assoc. It is claimed that this system gives the highest ratio between number of cars parked to the area of slabs built. All ramps are one way.

# EVENTS AND COMMENTS

## APOLOGY

**I**N my special congratulations on the M.O.H. Housing Awards I omitted the name of Clifford Culpin, who also received two awards.

## FESTIVAL SOUVENIRS

**A** LETTER to *The Times* from Sir Harry Brittain on the subject of souvenirs for the Festival of Britain brought a quick answer from Mark Hartland Thomas of the C.O.I.D. You will remember that the Festival authorities decided some time ago to sell only approved souvenirs within the South Bank exhibition and that they invited manufacturers to submit designs for approval. This was some four months ago. Sir Harry Brittain, having heard nothing since, then wanted to know what was going on. The C.O.I.D. reply said that things were going more or less to plan and that so far about four hundred designs had been submitted by 150 firms. About half of the designs had been provisionally accepted. Although some of the designs are very good, there are not many new ideas and the Council would welcome original ideas. The designs cannot be made public at this stage for fear of piracy, but, according to a report in the *Daily Telegraph*, there will be an exhibition of souvenirs before the Festival opens.

## AMERICAN VIEW

**A** CREW-cut American student was ushered into my office the other day and, after questioning me closely for about half-an-hour on the organisation of the British building industry, said that it might surprise me to know that in some aspects of industrial relations this country was far ahead of the United States; and, furthermore, that it was considered in America that there were some fine brains in this country who were, as they were wont to say in America, "right on the ball." "Oh," I said, "you mean that we are not after all finished?" "No," he said, without the glimmer of a smile, "Not by any manner of means."

## IT ISN'T CRICKET

**I** SELDOM watch cricket, and then only at Canterbury, where the sweep of the white tents with their gay flags, the elms and chestnuts, and the incomparable turf combine to give me a sense of quiet well-being which I never feel elsewhere. Architecturally the ground is ruined by the vast reinforced concrete stand built between the wars, which is completely out of scale with the other buildings. However, by sitting in it one does not see it, and it provides a good view. This year my calm was a little upset by the Wall's ice-cream trailer illustrated in my picture. Wall's can almost be counted among the country's benefactors, but perhaps not quite. Examine the picture closely or you may miss the stained glass insets with heraldic devices in the front of the counter. Note particularly the detailing of the columns which allows them to be equally efficacious should the trailer be capsized in a ditch. The steps, which of course are purely utilitarian, give the air of a temple and perhaps give a clue to the origins of the idea. The nearby photograph of the palladian bridge explains what



photo: P. F. Wiggins

I mean. The likeness to a temple is accentuated by the *prie-dieu* in the centre which contains the wheels. This effort must be classed with the Tavern Cars. The trailer shop is a very good idea but a chance of doing something really well has been missed again. This should be a case of "Stop me and sell this one."



## PRIOR PARK PALLADIAN BRIDGE

**T**HE Palladian Bridge at Prior Park, Bath, designed by John Wood the elder, is to be taken over by the Ministry of Works. This action comes not a moment too soon, for the bridge is in a very bad state, as my picture shows. I hope that the M.O.W. will also restore the fishponds so that there is some water for the bridge to span, and be reflected in.

## ARCHITECTURAL CRITICISM FROM THE U.S.S.R.

**A**N indulgent reader has sent me a copy of a bulletin of the S.C.R. for the U.S.S.R. It is the translated report of an interview with I. V. Zhiltovsky, a Member of the Academy of Architecture, who was recently awarded a Stalin prize for a block of flats in Moscow. Apart from the fact that the word Soviet appears rather frequently, the interview might have been with almost any academician about twenty years ago. It is incredible that it should have been thought worth while to circulate it.

"Ivan Vladislavovich," says the interviewer, as a sort of coda, "You have made frequent trips abroad. What is your opinion of the modern architecture of Western Europe and America?"

"It is clear to me," says the master, "that we have nothing to learn from them. Russian architects have always gone their own way. The beauty of the Tower of the Saviour in the Kremlin, and of the Cathedral of





*The stretch of the Grand Union Canal, known as the Pool, in Paddington. The Paddington Borough Council proposes to build a block of flats on the east bank, but the L.C.C. wants it to be maintained as an open space.*

Basil the Blessed, are our native Russian creations. Since olden days we have had our main architectural traditions created by the people.

"I respect and appreciate the English architects Inigo Jones and Christopher Wren, the creator of St. Paul's Cathedral. But I certainly don't like what the English are doing to-day. The buildings look like slide rules. Glass, glass, and nothing else; neither form, nor idea, nor clear conception. I don't understand how people with such a high culture in the past can stand for these structures.

"As for America, it is giving us simple (*sic*) frightful things. With all my respect for American building technique, I cannot apply the word architecture to the American buildings. They lie outside the pale of art."

All this reminds me of F. R. Yerbury's famous story of how, after delivering a lecture on modern architecture in a northern British town, he asked whether there were any questions. After a pause a man stood up and said in broad Yorkshire, "If I had to live in a house designed by that Mr. Corbooser, I don't know what I'd do." I forgot to mention that Ivan Vladislavovich Zhiltovsky is eighty-four, and an Honorary Corresponding Member of the R.I.B.A.

#### GAS WORKS AGAIN

NO one will deny that the Gas Board exists primarily to give an adequate supply of gas to the country. To increase its capacity to do so it must often enlarge its installations. On the other hand no one except possibly the Board themselves will deny that gasworks are

among the most hideous of industrial installations. I have commented on their architecture before (this page, February 24, 1950). So far the Gas Board has appeared impervious to criticism and has shown itself to be committed to a programme of expediency rather than long-term planning. Our power stations are on the whole well designed and tidied up. Battersea set an example of the cathedral type of power station which has often been imitated but never bettered. Can the gas industry point to a gasworks which is not an eyesore? In fairness I admit that they have a difficult problem. Gas-holders will be gasholders, and existing gasworks have a way of being in the centres of towns. Wherever possible, however, they should be resited and expansion on existing sites where amenities are threatened should be forbidden.

The latest threat is to St. Albans, where it is proposed to increase the gas-making capacity of the works by more than one hundred per cent. Not, as might be imagined, to supply more gas to the city, but to provide a service for the new town of Hemel Hempstead and for Watford and Welwyn Garden City.

The St. Albans gasworks already spoils the view of the cathedral from the south-east, and the new installation would include a gasholder 150ft. high and other tall buildings, not to mention smoke, steam and smell. It is time that the full weight of public opinion was brought to bear on the Gas Board to make it realise that it has other responsibilities than the production of gas. Let them cut their foolish caperings with Treasure



Island and Alice and Wonderland and get down to serious business.

#### A CORNER OF PADDINGTON

A ROW of rather a different kind is going on in the correspondence columns of *The Times* over an open space overlooking the Grand Union Canal Basin in Paddington. The Paddington Borough Council wants to build flats on it, the L.C.C. will not give consent and the matter is now in the hands of the Ministry of Town and Country Planning. The site is at present occupied by two derelict houses and a few studios, and there is no doubt, as my picture shows, that it would make a charming waterside garden of the type which is most scarce in London. There is no doubt either that Paddington is sorely in need of sites for housing. Its need is so great that it must find space to move people into so that the accommodation they now have can be pulled down. It is claimed that the Council realised that the site in question would make an excellent garden, but considered that the need for housing was more pressing. The opposition says that expediency is being allowed to ruin planning again. The council replies that the flats will be raised on stilts so that some view of the water will be retained. There is clearly much to be said on either side and it is a pity that politics seem to have intruded.

Oddly enough, it is a Conservative Council which wants to build on the site.

A third view has been given by Mr. Feliks Topolski, who wants to turn the garden into an arts centre by rebuilding the studios and by making provision for a children's theatre and facilities for holding art exhibitions.

As with the caravan-dwellers, it all depends on one's point of view. Open spaces and art centres are all very well, but if I were trying to raise a family in a single room in a derelict building, I know what my views would be.

#### NOT HISTORIC ARCHITECTURE

ON June 2 I commented on the listing by the Ministry of Town and Country Planning, as a place of historical and architectural interest, of a 1947 converted barn in Essex whose owner described it as "an architectural stew." The Ministry has now decided that a mistake was made and it really meant somewhere else, and would the owner please return the notice. It may be inciting the owner to commit a misdemeanour, but I strongly urge him to do no such thing but to frame it, and if he has not already done so, to hang it in the appropriate place. We do not often get a good laugh out of the Ministry of Town and Country Planning.

A B N E R

## NEWS OF THE WEEK

### L.C.C. Schools Architect

Mr. Sydney Howard, L.R.I.B.A., who has been appointed Schools Architect in the Department of the Architect to the London County Council at the maximum salary for this post of £1,700 per annum, will take up his post on a date to be arranged. The General Purposes Committee of the L.C.C. are selecting a successor to Mr. Howard in the Housing and Valuation Department.

### Reilly Medal and Prize

As a result of subscriptions received from old students and friends of the late Sir Charles Reilly, "The Reilly Medal and Prize" has now been established at the Liverpool School of Architecture, the University of Liverpool made famous by Sir Charles Reilly, who was Roscoe Professor of Architecture there for 26 years.

The Medal and Prize of £35 will be awarded annually to the Fifth (Final) Year student whose Thesis design is considered the best, so that to win the Reilly Medal will be the crowning achievement of a student's career.

The Medal is in bronze, and is the work of Mr. H. Wilson Parker, Sculptor.

### Control of Outdoor Advertisements in Cheltenham

The Minister of Town and Country Planning has informed the Borough of Cheltenham that he will confirm an application to make certain parts of the Borough areas of special control for outdoor advertisements.

The order, as submitted, covered the whole of the Borough but at the local inquiry held on May 14 last the

amenity of Cheltenham the Minister finds no reason for disagreeing with them.

### R.I.B.A. Examinations

#### The Final Examination, 1950

The Final Examination was held in London, Birmingham, Leeds, Edinburgh, Manchester, Newcastle and Belfast from July 5 to 14.

Of the 406 candidates examined, 163 passed as follows:

Passed Whole Examination	51
Passed Whole Examination, subject to approval of Thesis	57
Passed Part 1 only	54
Passed Part 2 only	1

163

243 candidates were relegated.

### R.I.B.A. T.P. Diploma

Mr. Arthur Roger Pearce, A.R.I.B.A., has been awarded the R.I.B.A. Diploma in Town Planning.

### Law Report: Addendum

In a recent report of an application in the Law Courts involving the Incorporated Association of Architects and Surveyors and certain of its members it was not made clear that the application was dismissed.

The application arose in an action brought by four members on behalf of themselves and other members of the Association against the Association and Mr. E. St. Clair Browne and 38 other personal defendants.

The plaintiffs, in the motion which was dismissed, sought interim injunctions restraining the personal defendants or certain named defendants from acting as members of the Council which is



The Reilly Medal

Town Clerk withdrew the proposals for special control for certain commercial and industrial parts of the town.

No objections were made to these proposals and bearing in mind the

the managing body of the Association.

In dismissing the application, Mr. Justice Danckwerts said he was satisfied that there had been delay which precluded the plaintiffs obtaining interlocutory relief.

The points which had been raised were troublesome and the plaintiffs must fight their battle at the hearing of the action. The only order he would make on the motion would be that the costs would be costs in the action.

## From the Minutes of the R.I.B.A. Council Meeting

At the meeting of the R.I.B.A. Council held on July 25, the following appointments were made:

(a) *R.I.B.A. Representatives on the Architects' Registration Council of the United Kingdom:* Mr. J. T. Castle and Mr. T. E. North in place of Mr. Martin S. Briggs and Lieut.-Colonel V. H. Seymer.

(b) *Building and Civil Engineering Regional Joint Production Committee for Region No. 12:* R.I.B.A. Representative: Mr. H. Edgar Bunce in place of Mr. Cecil Burns.

(c) *International Union of Architects—Assembly Paris, October, 1950:* Delegates of British National Committee: Mr. Anthony Chitty and the Hon. Godfrey Samuel.

(d) *National House-Builders' Registration Council:* R.I.B.A. Representation: Mr. C. H. James re-appointed. Note: the other two representatives are Mr. A. W. Kenyon and Mr. K. J. R. Peacock.

The R.I.B.A. Council have approved recommendations made by the Institute's representatives that the time limit for the completion of the consideration of comments on outstanding draft Codes of Practice be extended to June 30, 1951.

On the recommendation of the Competitions Committee the R.I.B.A. Council have approved the draft of a model form of agreement between a promoter and the successful competitor in an architectural competition. The form will be printed and will them be available on the same basis as the model forms of agreement between a client and an architect for general purposes, for housing and for flats.

## Exhibition of Competition Designs

In view of the continued difficulty in regard to accommodation the R.I.B.A. Council have approved a recommendation of the Competitions Committee that the following footnote to Clause 8 of the Regulations Governing the Promotion and Conduct of Architectural Competitions should continue in force for a further twelve months from July 1, 1950.

"The large numbers of entries received in one or two recent competitions and the restricted accommodation available under present circumstances have made it difficult to comply with the terms of Clause 8 of the Regulations for the Promotion and Conduct of Architectural Competitions, and Clause

15 of the Model Form of Conditions relating to the exhibition of competition designs.

"The Council, on the recommendation of the Competitions Committee, have agreed to relax this requirement for the time being. Where there is insufficient accommodation to show all the designs at one time it is suggested that they should be shown in relays for at least six days, but that the premiated and commended designs should be shown throughout the exhibition. It is important that each competitor should be notified when and where his particular design will be shown."

The R.I.B.A. Council have approved a number of comments and suggested amendments to draft regulations for International Architectural Competitions. The draft regulations had been submitted by the International Union of Architects and had been considered by the R.I.B.A. Competitions Committee. The principal amendments proposed were in connection with securing the complete anonymity of competitors in accordance with the British practice.

## APPOINTMENTS

Mr. Francis Sidney Drake, A.R.I.B.A., of Culford Road, Dalston, London, N.1, has been appointed an architect in the Public Works Department, Tanganyika.

Mr. James Nisbet Lockerbie, A.R.I.B.A., Edinburgh has been appointed an assistant architect in the Public Works Department, Malaya.

## PARTNERSHIPS

Messrs. Lubetkin & Skinner, formerly of 115 Mount Street, W.1, have taken Mr. D. C. Bailey into partnership and will in future practise as Messrs. Skinner, Bailey and Lubetkin, at 188 Piccadilly, W.1, telephone Grosvenor 6795/6.

Graham Dawbarn and R. F. Lloyd Jones (Norman and Dawbarn) have taken into partnership Frank Rutter as from April 1, 1950. The Associate members of the firm are Alick Low, Herbert Wilson and Raymond Pidgeon.

## CHANGE OF ADDRESS

As from September 29, 1950, John Grey & Partner, Chartered Architects, 56 Oakley Street, Chelsea, S.W.3, will be practising from 17 Buckingham Street, Strand, W.C.2. Telephone: Trafalgar 3787.

## LETHABY SCHOLARSHIP

The 30th of this month has now been fixed for the closing date for applications for the Lethaby Scholarship founded in memory of the late Professor W. R. Lethaby to enable architectural students to study the methods of repairing old buildings used and approved by the S.P.A.B. Full particulars of this Scholarship appeared in A. & B.N., June 30, 1950. Details are obtainable from the Secretary, Society for the Protection of Ancient Buildings, 55 Great Ormond Street, London, W.C.1.

## CORRESPONDENCE

### Responsibility for Housing

To the Editor of A. & B.N.

Sir,—On page 148 of your issue dated August 4, 1950, you state that the Secretary of State for Scotland with a stroke of the pen has done wonders in the employment of Registered Architects upon the greater responsibility of Housing progress.

I wish to combat you in your views regarding the R.I.B.A. and that they should now take up the matter with London and district, and to point out that although I am only a Surveyor, I have been on the direction of housing and building for a number of years both on my own account and when a Registered Architect has been in charge, and my views are as follows:

(a) That in numerous instances where Registered Architects have been employed on jobs that I have been connected with, I have only seen the Architect at very widely spaced intervals, on the last building it was three times during the erection of a house, which took about seven months to build.

(b) I entirely disagree with you, in as much as a Surveyor who is on the actual job and knows just what the local authority wants is in my opinion the man to draw up plans for submission to the local authority providing they are drawn up to standard.

Surely we have had enough of this banding together of the selected persons and that this democracy of ours should not be dictated, or the powers of dictation handed, to authorities to stipulate that only Registered Architects should draw plans; surely no matter what a man is, so long as he is good enough to meet the requirements as good, probably even better, than others, then he is the man to do the work and not a body which is nominated.

I am, etc.,

A. H. JARVIS, A.A.B.S.S.



## The Zuidplein Flats

To the Editor of A. & B.N.

Sir,—Touring through Holland during August 1949 brought me to Rotterdam. There I was shown the block of flats in the Zuidplein, as illustrated in the A. & B.N. of August 4, 1950.

A clear photograph of the treatment of the Mijnsheerenlaan elevation is not included in your illustrations, and this has prompted me to enclose one of my own prints of this interesting building.

I am, etc.,

L. A. WAKEHAM.

## MINISTRY OF WORKS REPORT FOR 1949

The Ministry of Works Report for the year 1949 has just been published (H.M.S.O., price 1s. 3d.).

The following is a brief outline of matters dealt with.

Building labour was not the main limiting factor in the amount of work that could be carried out last year under the National Building Programme. The quota of building labour given in previous years to Government Departments to serve as a guide to Departmental "ceilings" was therefore discontinued.

Tables included in the Report show the strength of the building labour force according to trades. For the first time these are categorised in two groups: Building and Civil Engineering trades ("the Thirteen Trades") and Specialist trades ("the Seven Trades"). In the labour field, shortage of bricklayers was a problem throughout the year in England and Wales; in Scotland lack of plasterers and joiners was the principal difficulty.

Other tables deal with the output of the Building and Civil Engineering Industry quarter by quarter, the value of authorised and licensed work, unemployment, apprentices, production and stocks of building materials and fittings, and the quarterly price index for the years 1946-49. This last (Table XI) shows that the general level of prices remained virtually unchanged during the year although there were variations in prices of certain individual items.

### Building Licensing

During the year, 192,174 individual applications for building licences were made to the Ministry, and licences to the value of £258,401,000 were issued; 51,537 applications for annual maintenance licences were also dealt with, the total value thus issued being £25,571,140. Licences issued by Local Authorities for housing work (other than erection of new houses) were 182,695, representing a total value of £54,658,000.

### Consultative Machinery

A number of changes were made during the year to strengthen the machinery of consultation. The Headquarters Building and Civil Engineering Joint Committee was abolished, its functions being transferred to the National Consultative Council. Outside London, Building and Civil Engineering Joint Committees were strengthened by the inclusion of representatives of the R.I.B.A. (in Scotland the Royal Incorporation of Architects), the Institution of Civil Engineers and the Royal Institution of Chartered Surveyors—bodies already represented on the National Consultative Council. A new Consultative Committee for the Stonemasonry Industry was set up.

### Publicity

In its account of exhibitions, publications and other measures taken towards disseminating information on research and development, of which the Building

Week at Tunbridge Wells (forerunner of the bigger "Building Week" at Leeds next September 11-16) was an outstanding example, the Ministry pays a tribute to the publicity given by the Technical Press.

### Building Materials

The chapter on Building Materials records a setback to asbestos cement production owing to world shortage of raw asbestos fibre. The demand at home for sheet glass fell very sharply in the middle of the year, causing the closure of one plant. But the year's most difficult problem was in the production of bricks. With a previous fall in the labour supply, stocks had accumulated. Later, the demand for bricks throughout the mild winter of 1948/49 exceeded the reduced level of production and early in the year the Ministry was called upon to assist in finding additional labour. Although the post-war record output of 483 millions was attained in November, the very high level of demand was never quite balanced until December.

Gas cooker manufacturers started production at a price enabling gas undertakings to reintroduce hire-purchase schemes for lower income groups.

### Building for the Government

Account is given of the Ministry's work as the Government's principal building agency. At the end of 1949 it was looking after about 23,500 buildings and had spent nearly £11,500,000 on their maintenance during that year. Some £30,000,000 had also been spent on the erection of new buildings and on the adaptation of existing buildings.

But it was not all "take" and no "give". During the year 2,232 requisitioned buildings were handed back to their owners, resulting in the release of some 5 million square feet of floor space, 1½ million of it in London.

Some 436,000 square feet of single storey office buildings were completed during 1949 and another 400,000 square feet put under construction.

A new requirement was for Valuation Offices for the Department of Inland Revenue, for rating valuation work previously carried out by local authorities. About 300 offices are being made available for transfer of this work on the "appointed day".

For the postal and telephone services 77 new buildings, costing nearly £1,000,000 were completed. A further 102 (estimated cost £2½ millions) were started.

Besides designing new office furniture for general service use—involving new schemes of decoration and lighting—the Ministry co-operated with the Department of Inland Revenue in designing special furniture and equipment for P.A.Y.E. sections of Tax Offices. In collaboration with the Ministry of Labour, a new layout for better reception and interviewing in Employment Exchanges was prepared,

and the first public office of this type was completed at Brook Green, Hammer-smith.

Many other examples of the varied building tasks undertaken by the Ministry will be found in the Report.

### Facts and Figures

For the Centenary Pilgrimage along the Roman Wall, Northumberland, the Ministry erected a temporary bridge across the River Irthing so that visitors could follow the line of the wall.

More than 4,360,000 people paid £98,000 for admission to Ancient Monuments, Historic Buildings and Royal Palaces under the control of the Ministry. The most popular was the Tower of London, with 897,000 visitors who paid £22,000 in admission fees.

State Apartments and Swiss Cottage, Osborne, were visited by more than 60,000 of the paying public, in addition to 5,000 schoolchildren who were admitted free. 20,000 people visited the newly-opened State Apartments, Kensington Palace.

The Ministry has charge of some 1,700 pictures in Government buildings about 300 being in Embassies and Legations abroad. There has been a disappointingly small response from private owners to the Minister's appeal in Parliament for the loan of pictures.

About £1½ million was paid in final compensation for gates and railings requisitioned during the war.

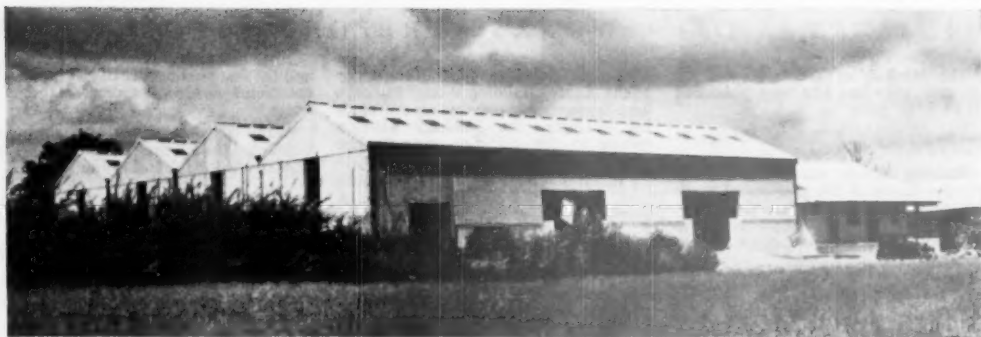
Nearly 15,000 contracts were made, to the value of £28,000,000.

Crockery requirements for Government needs (including school meals) were about 250,000 pieces a week. It was not possible to obtain deliveries at that rate. To help meet the deficiency large quantities of plastic plates and beakers were purchased for the school meals service.

Mr. A. G. Shoosmith, acting for the Ministry of Town and Country Planning, has closed the public inquiry on the proposals of the Eastern Gas Board to enlarge the St. Albans Gas Works. The decision of the Minister will be promulgated.

It is expected that work will begin this autumn on the new town of Peterlee. The first tenders have been received, we understand, covering roads and sewers for 1,000 houses, and for the construction of the first 250 houses.

Other houses will be added as the new town receives its allocations. The original population planned for was 30,000, but Lord Beveridge, the Chairman of Peterlee Corporation was reported in *The Times* as saying "we shall have to see how the town grows". Lord Beveridge also said that he would like to begin before long the building of a technical college.



DAIRY FARM, GREAT CRANFIELD, ESSEX

THIS article illustrates a new system of dairying which has been carried out at Ashfields Farm, Essex. The accommodation, which is based on six straw yards, is to house 72 dehorned Ayrshires and their followers which will comprise the stock for 260 acres.

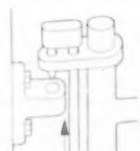
In the building where milk production takes place the feeding stuffs, forage, and the machinery for their preparation are arranged on one side, facing the greatest number of yards and feeding troughs, while the office is almost in the centre of the milk production area, with lactory and future calf house, wash room, cold storage and milk dispatch, etc., all close by. In the roof on one side of the lactory is a viewing room for visitors who may see milk production in progress throughout the building without disturbing the cows. Straw for litter can be drawn by tractor straight through the yards.

On the undrained concrete floors up to 3 ft. of muck can accumulate when mechanical loading and carting will present no difficulty.

Cattle are circulated between yard and lactory by a variety of routes while steel gates in the yards are so arranged as to make it possible to drive cattle between any of them. Ingenious combined hinges and drop hooks are used at each end of the gate so that they may swing to either side from either end.

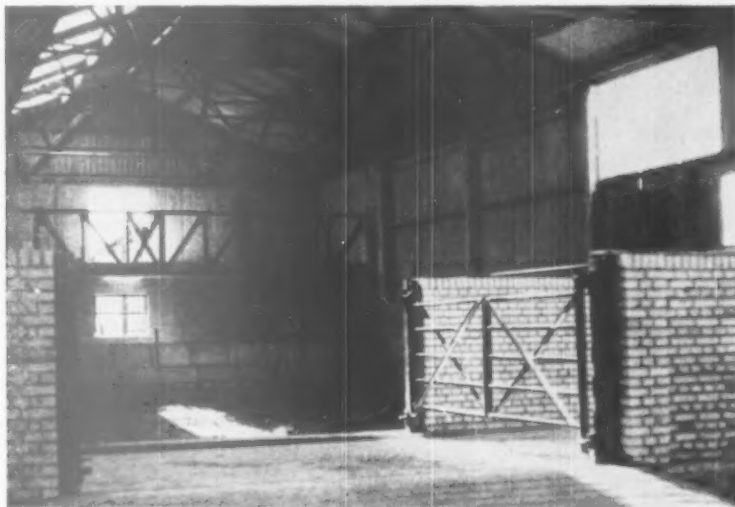
For the construction of the building M.A.F. roof trusses and components have been used with certain modifications. A steel surfaced floor in the dairy provides the hard wearing finish which is extended to the ramp for loading. Steam is provided by a Perkins oil-fired vertical boiler. The milk storage room is air-conditioned, and the floor is cooled underneath. Rain-water from the valley gutters in the roof is stored and used for drinking purposes. Mains water is available for washing up and is circulated around the building above ground in pipes lagged with glass wool. The building could be altered with the minimum of trouble for any other form of production; only the dairy and lactory would be out of use.

General Contractors for the building were Messrs. Walter Laurence & Son.



Detail of combined hinge and latch.

The adaptability of the gate hanging enables two yards to be joined across a gangway.



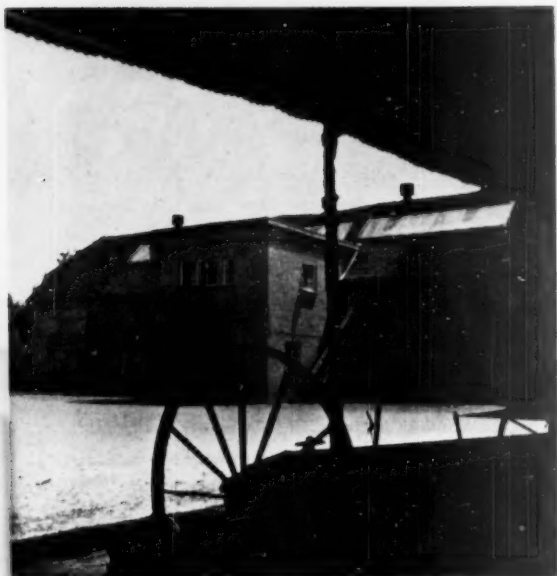
architect: GERALD LACOSTE, M.B.E., A.R.I.B.A.



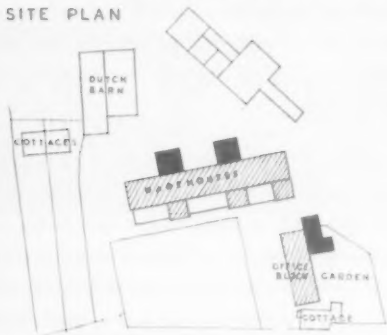
## WAREHOUSE AT HENLEY-ON-THAMES

architect

RACHEL CARO, A.R.I.B.A., A.M.T.P.I.



SITE PLAN



**R**OUND these two fine flint and brick grain barns a prosperous farm had developed, but owing to the encroachment of the town, it was gradually being swamped. The result was, that the barns were falling into disuse and disrepair, whilst a much smaller farm was still being carried on round them.

The project was to convert the larger of these barns into a warehouse for textiles, and the other into offices; and this in itself was an experiment in the decentralization of warehousing these goods outside the metropolis.

The project was undertaken when both timber and steel were particularly scarce, and these materials had to be spared wherever possible whilst town planning permission for a warehouse in this rural siting was only permitted if the character of the buildings externally was left unchanged.

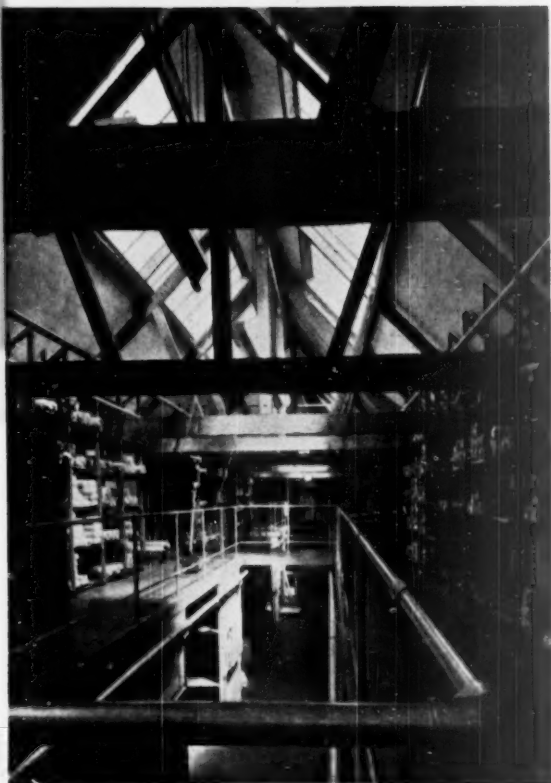
As well as a separate office and warehouse block, the scheme provided for two semi-detached cottages sited adjacently to provide accommodation for two of the staff with their families.

The warehouse is planned with two packing rooms to separate the "ingoing" and "outgoing" goods, whilst a separate entrance for the staff complete with cloak-rooms is provided on the same side as the office block.

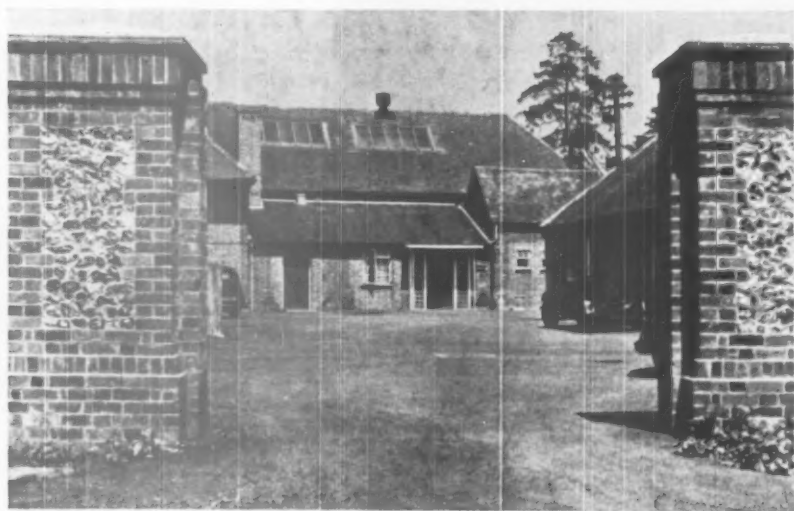
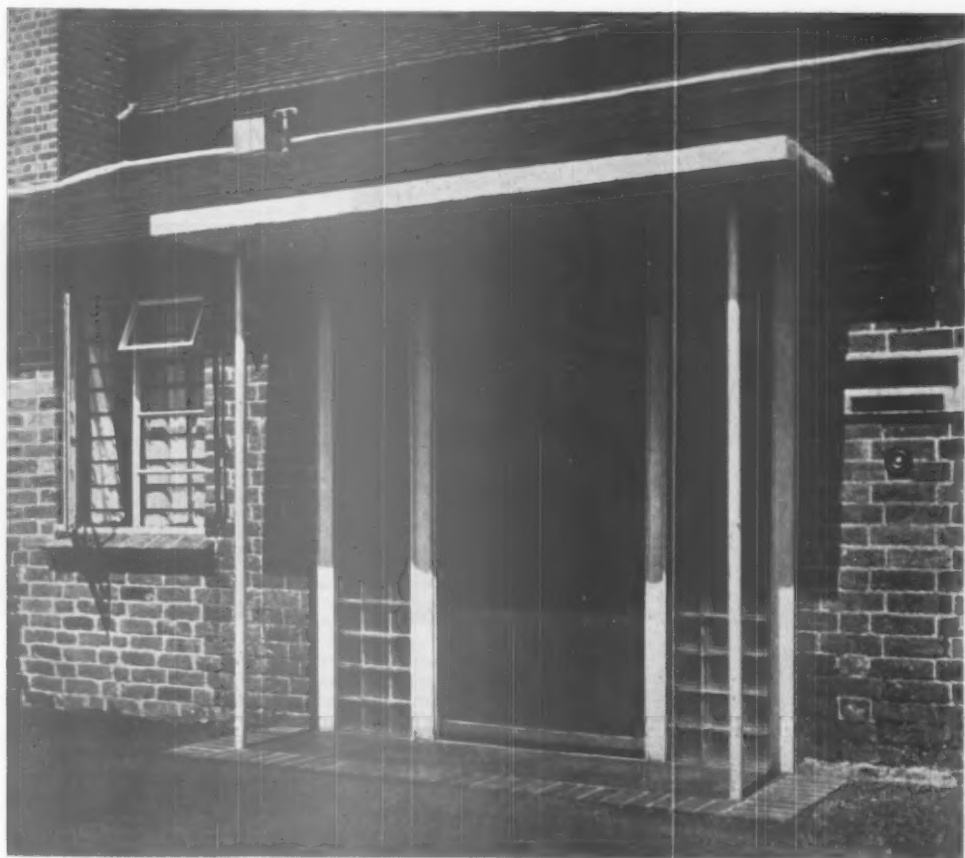
Owing to the inadequate internal height of the barn to form the warehouse proper; from the ground to the fine oak king post roof trusses, which, at approximately 14 ft. intervals, support the tile roof; it was not possible to construct two complete floors as the lower would have been too confined and dark. So a gallery floor was made, and on this upper floor the shelving was fixed against the walls with a railing round the 6 ft. internal well. This floor was supported by stanchions of various sections, which were also drilled at 6 in. intervals to carry the ground floor adjustable aluminium shelves, housed in two rows at right angles to the walls on either side of the warehouse. Between these bays, steel windows were inserted, whilst Haywards roof lights were fixed on alternate sides between the roof trusses, lighting both floors.

To enable the silent movement of the goods, the floor to the main warehouse was covered with 12 in. square cork tiles, and the Slingsby trolleys are rubber shod.

Heating is by means of oil burners thermostatically controlled, supplying radiators set well up on the ends of the ground floor shelving racks, to be away from the trolleys, as well as continuous exposed pipes at gallery floor level and truss level. This system is regulated by cocks to provide central heating when desired to the office block, supplied by means of underground piping.







WAREHOUSE, HENLEY - ON - THAMES

# PAIR OF COTTAGES FOR WAREHOUSE STAFF

architect

RACHEL CARO



The cottages are built in 11 in. brick with old facing bricks. The roof is finished with hand-made sand-faced pantiles.



## SOME RECENT WORK BY THE ITALIAN STATE RAILWAYS

THE Italian State Railways have a vast reconstruction programme in hand embracing every side of their transport problem. Many bridges are being rebuilt particularly those destroyed by bombing on strategic routes such as the Brenner Pass, but the most interesting side from a purely architectural viewpoint, is the reconstruction of their stations.

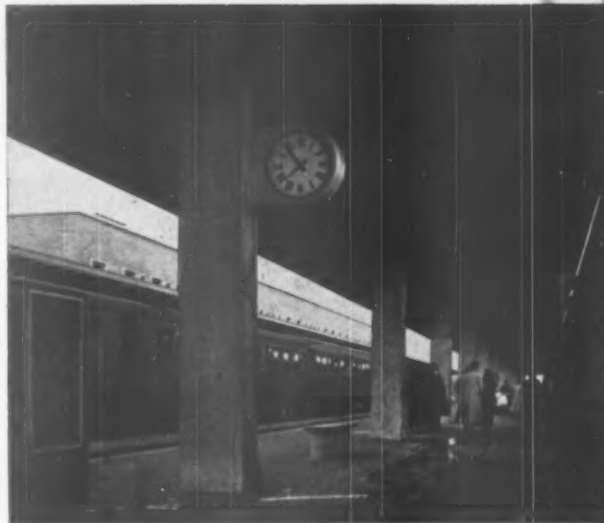
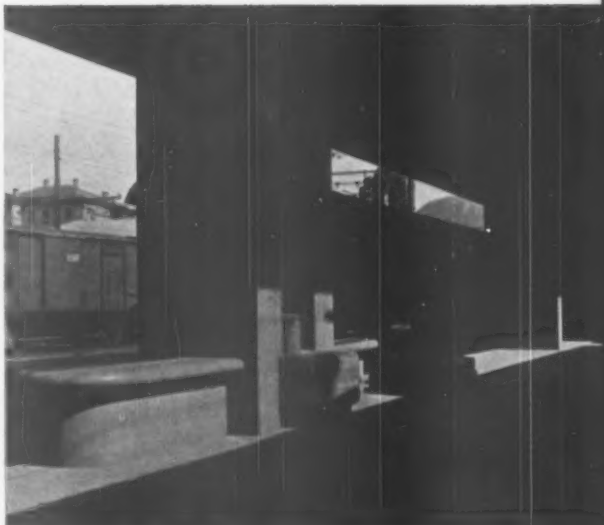
Generally speaking the planning of these buildings is on a generous scale each with a fine spacious concourse and wide platforms. Restaurants and waiting rooms, etc., are pleasantly designed with particular attention being given to choice of materials and good detailing.

Verona station is a typical example. The main platform and offices have so far been reconstructed and form a striking contrast with the remainder of the old-fashioned station.

As can be seen from the photographs, a wide reinforced concrete canopy penetrated with panels of pavement lights and supported on marble covered columns, covers the main platform. Walls are also faced with marble and a contrasting colour of the same material is used to line the door reveals to the various offices, etc. In the case of Verona, the upper floor of the offices is faced with a pleasant dull red coloured brick with white precast concrete linings to windows. The large window at the end of the main building lights the restaurant; meals can be taken either inside or at tables on the platform. The coloured tubular chairs and clean (yes, I mean it!) tablecloths give a pleasant light-hearted touch to the platforms which is seldom seen in stations in this country.

The detailing of seats, clocks and notice-boards is good and should be studied by those engaged on the reconstruction of our stations. Admittedly the Italians are fortunate in having a wealth of natural hard-wearing materials suitable for the requirements of this type of building, but I do think a lesson can be learned from them and that is, that the constant upkeep required to keep paintwork looking clean and fresh should be avoided and natural coloured materials should be used whenever possible.

NORMAN WESTWOOD



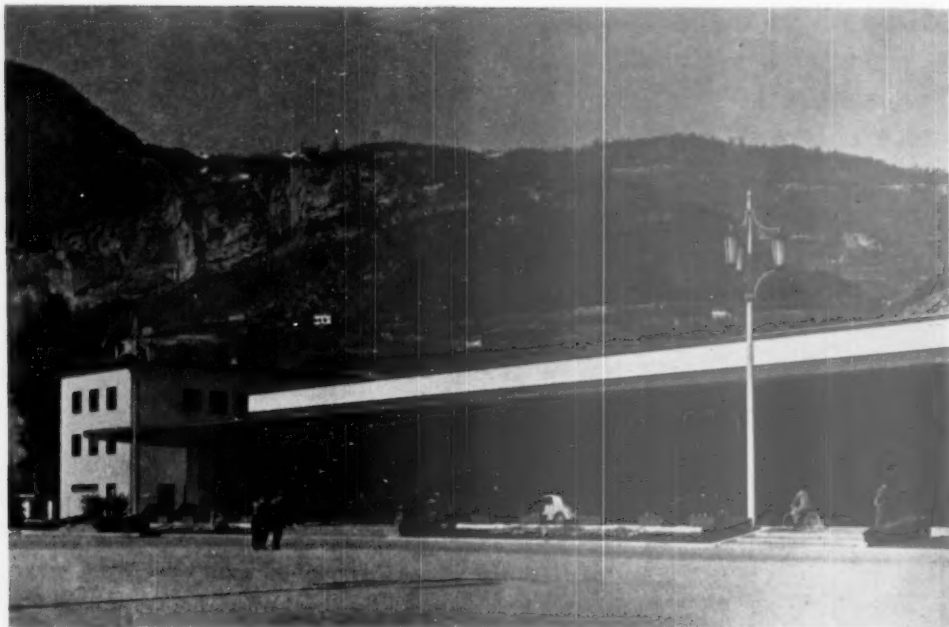
Top: TRENTO. View on one of the platforms showing a typical seat carried out in a honey coloured marble. The supports to the canopy are in the same materials and the underside of the canopy is finished in blue mosaic.

Below: View on one of the platforms at Venice station showing a detail of the usual design of clock used. In this case a pink coloured mosaic is used for the canopy.

Left: Taken in passing! A signal box just outside Venice station. Perhaps not too happy from a design point of view, but nevertheless a move in the right direction.

The photographs on this page and on pages 194 and 195 are by the author.

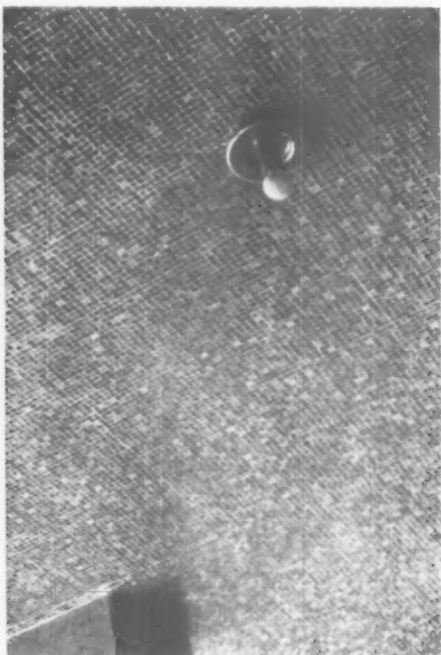




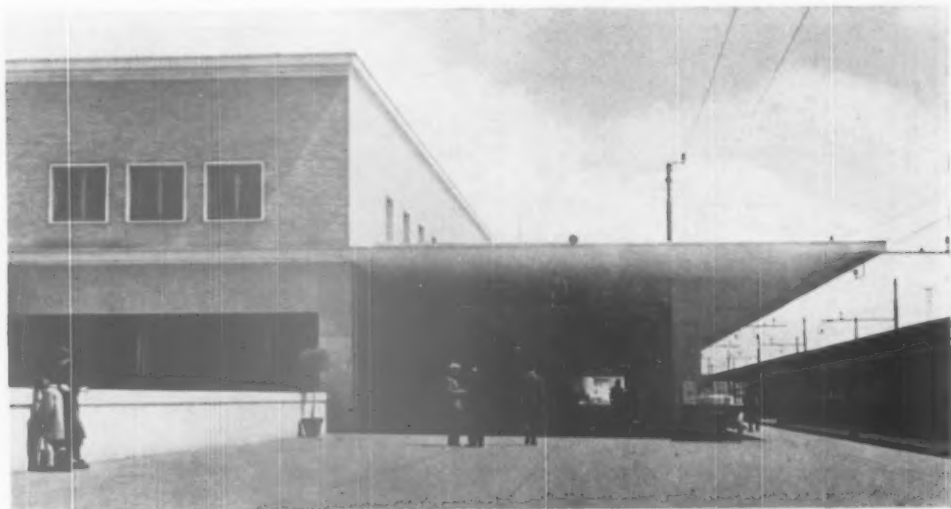
A general view of Trento station from the station yard. The canopy is faced with a dark green mosaic and the supporting columns with a polished blue-black marble. The lamp standards are concrete with metal lighting boxes glazed with opal glass.



TRENTO. A view under the canopy to the main entrance. On the ground, bands of polished marble link the supports whilst the infilling of the spaces between is carried out with granite sets laid to a circular pattern.

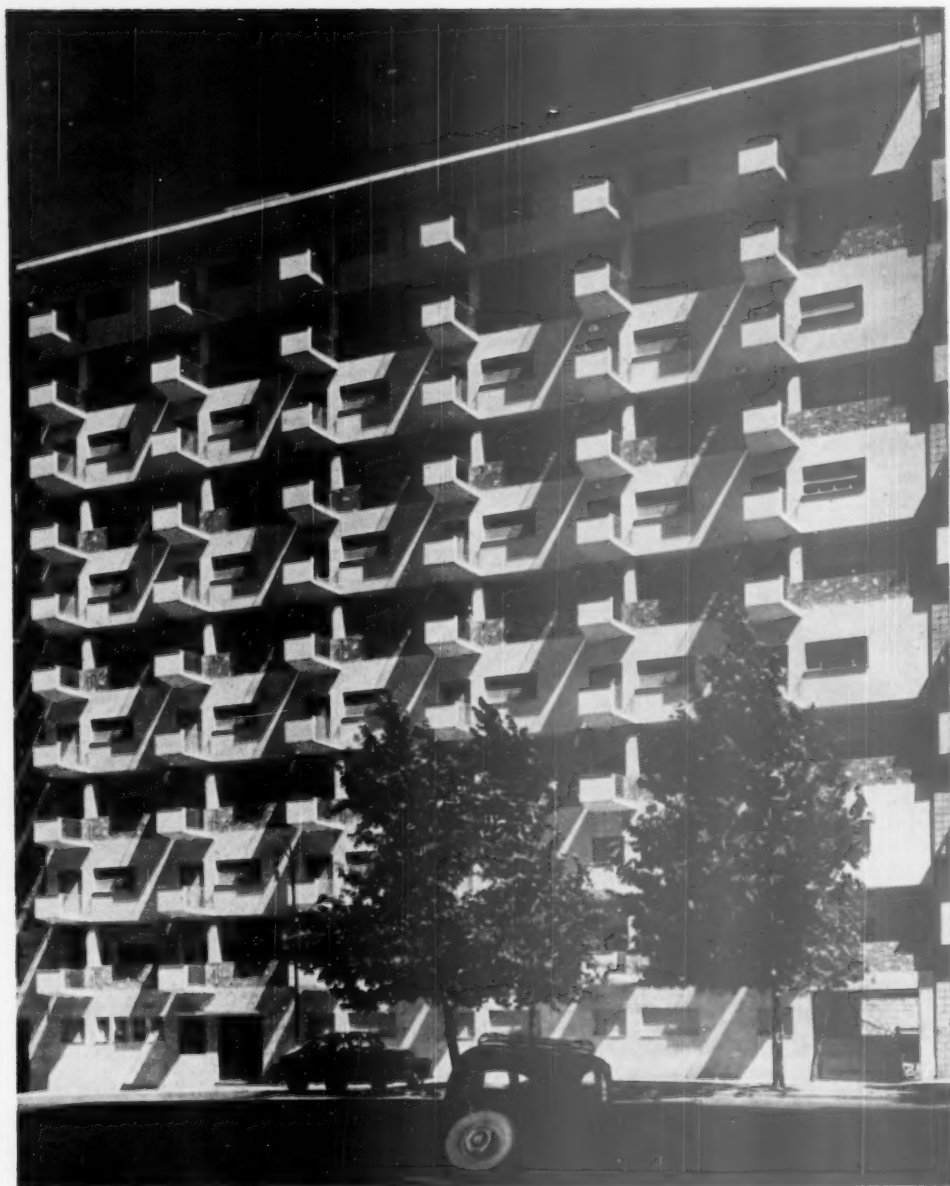


TRENTO. A detail of the mosaic to the underside of the canopy showing the neat light fittings that have been specially designed for the job.



Verona Station showing the main platform and offices after remodelling. Note the good detailing of signs, etc.

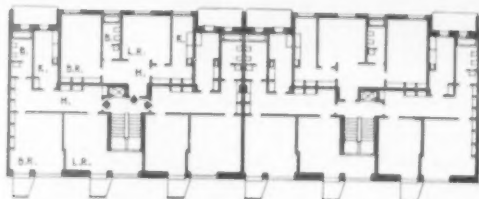
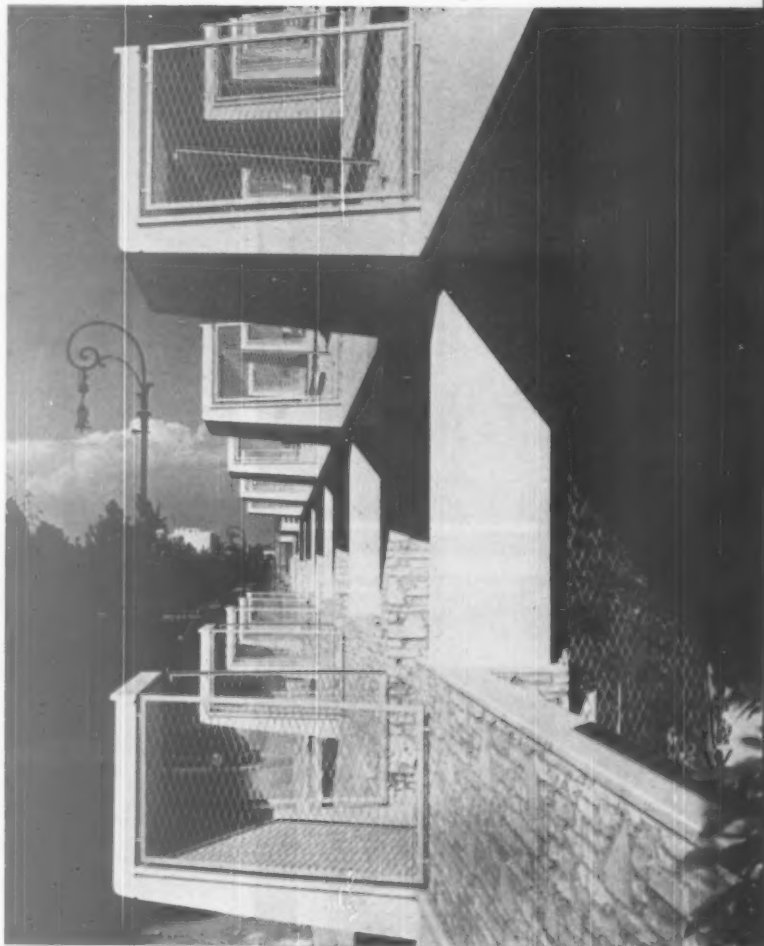
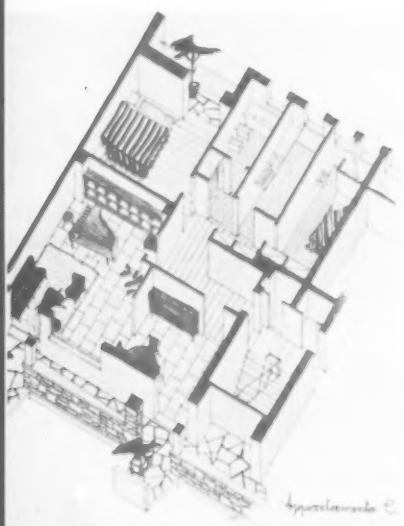
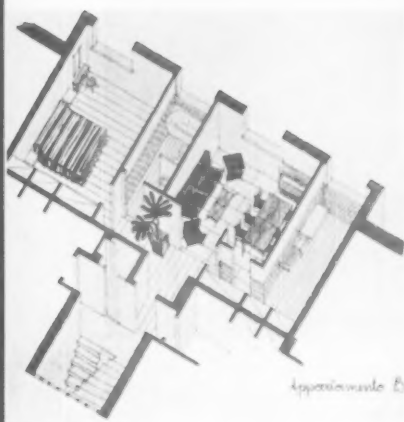
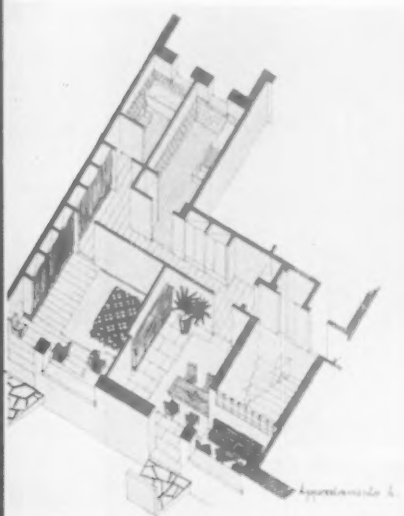
SOME RECENT WORK BY THE  
ITALIAN STATE RAILWAYS



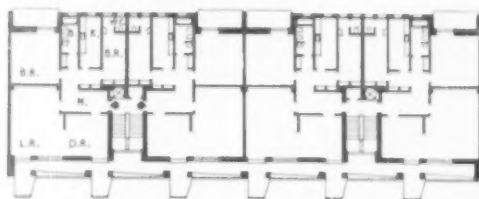
*This new block of flats in Viale Tinturicchio was completed this year. The three types of flat are shown on the page opposite, two are of 2-rooms and one 3-rooms. The aim of the architect was to avoid monotony of design associated with working-class flats. He has produced a bold pattern, marred by the self-conscious rubble walls and the crazy paving of the balconies.*

ITALIAN WORKING - CLASS FLATS  
 architect: DR. UGO LUCCICHENTI

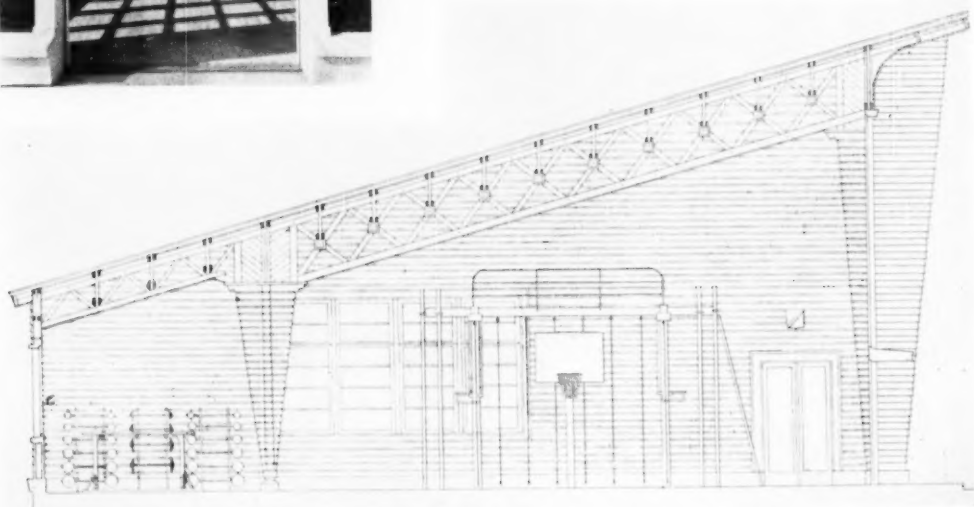
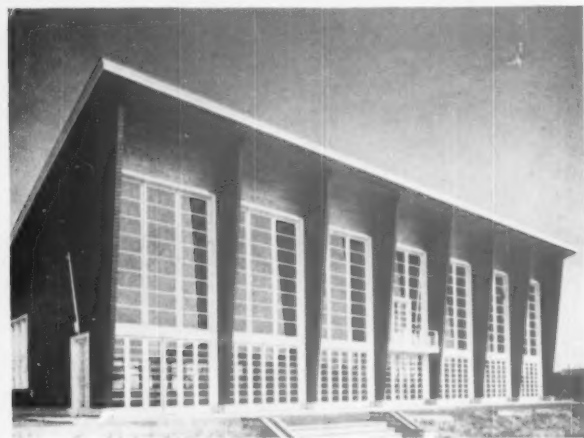
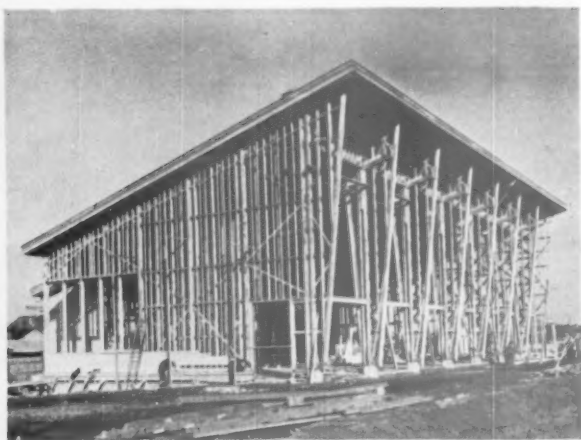




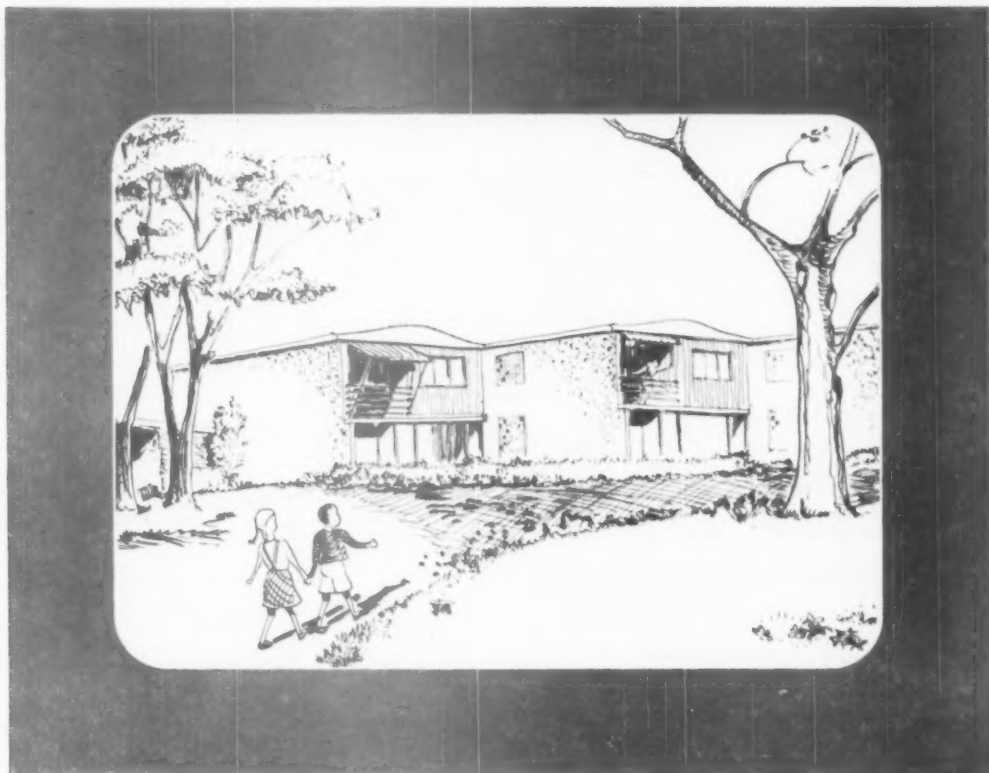
FLOORS 2·4·6·8·10



FLOORS 1·3·5·7·9



GYMNASIUM AT PAVILLONS-SOUS-BOIS CONSTRUCTED ENTIRELY OF PINE. ARCHITECT: H. LARRIEU, D.P.L.G.



## S I N G L E   S P A N   H O U S E

Described by the designer, H. W. ROSENTHAL, DIPL. ING., A.R.I.B.A.

**T**HIS article represents an attempt at solving the problem of housing in a positive way as opposed to the "negative" of seeking after ways and means of reducing standards. The design is intended to illustrate certain principles and ideas, without claiming to be the answer. For instance, there is very good reason in this version for the curved roofs, but other roof forms would be easily possible. The same applies to the staggering of the units.

As labour is by far the greatest item in building cost the aim should be to reduce this, particularly the skilled. Site work which is often unduly costly and laborious should be equally simplified. This could be achieved without resorting to prefabrication, although far greater numbers of building components could be made in the builder's shop. Houses should be sufficiently carefully detailed and dimensioned that "cut and waste" becomes negligible, which would save in labour and material. Skilled work should be reduced to a minimum by avoiding complicated walling, bonding, framing, etc. Carpentry should be straightforward, avoiding intricate joints, etc., and roofs constructed without all those items which usually make for structural complication coupled so often with inharmonious and meaningless design, such as valleys, gutters, hips, etc.

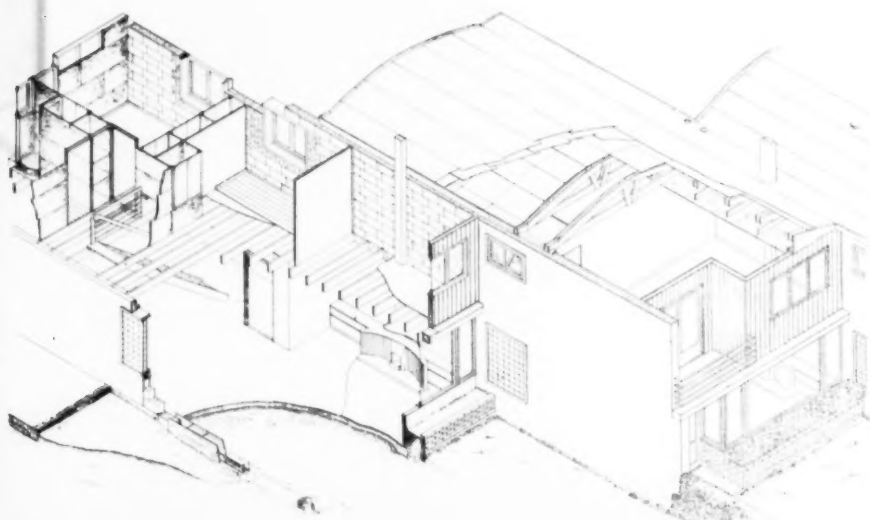
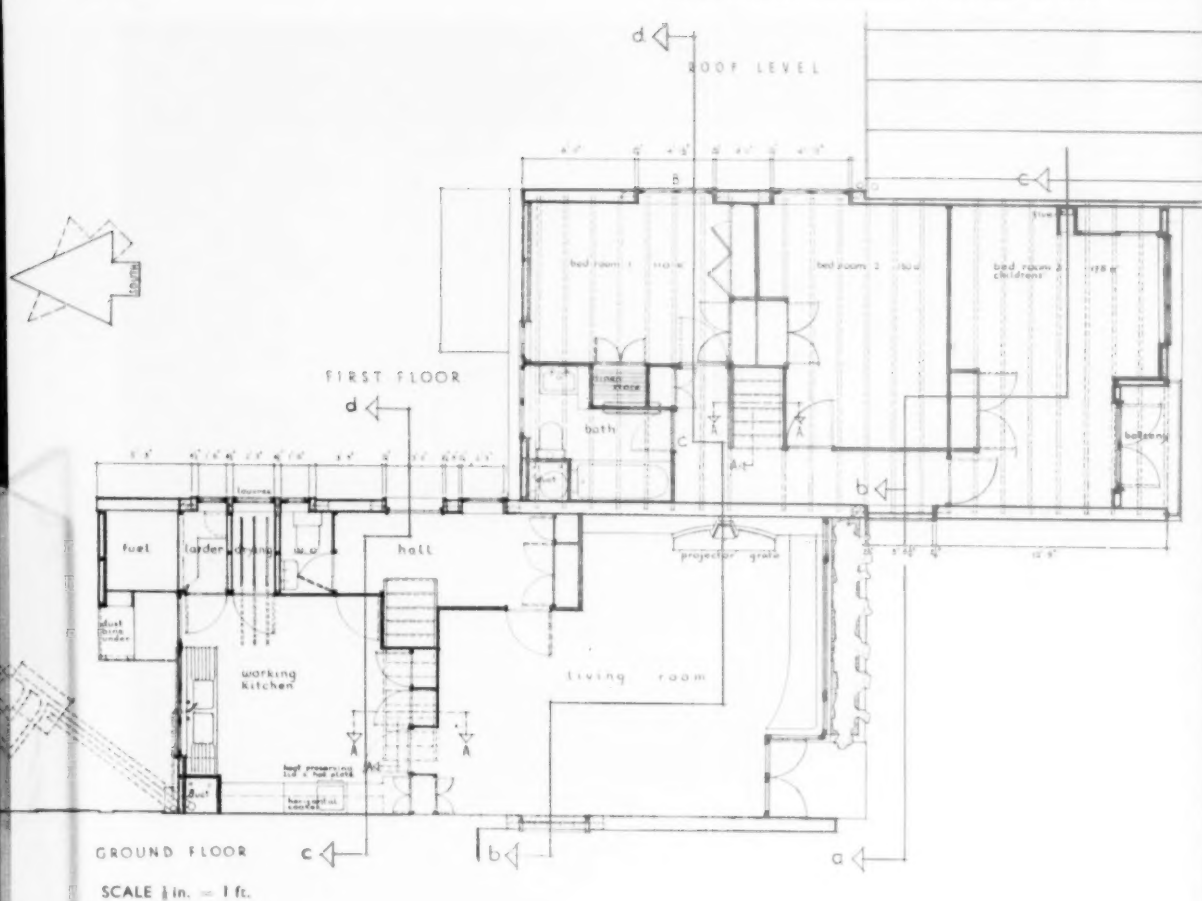
Constructional methods should be employed which would reduce to a minimum the moisture introduced into the fabric in the first place to shorten drying out periods and waiting times and incidentally many sources of failure.

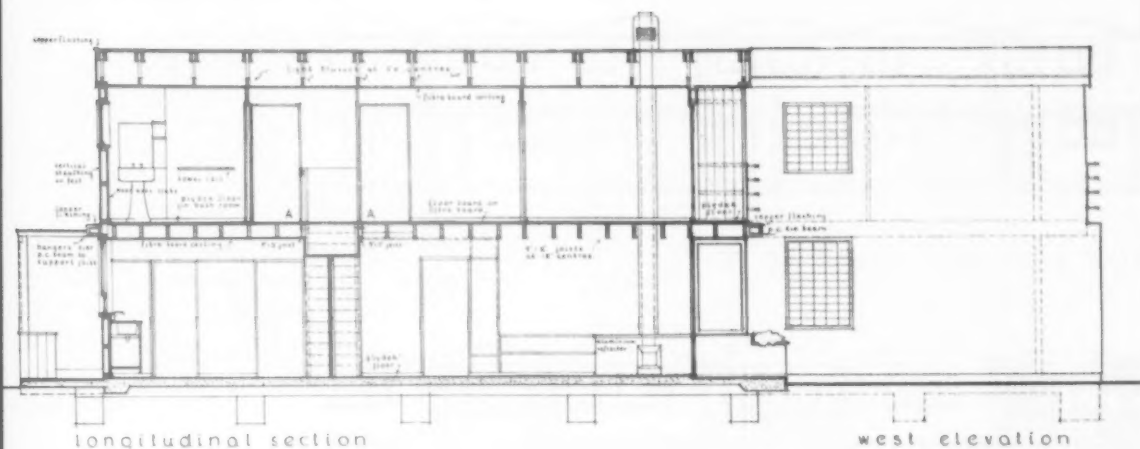
Generally speaking, housebuilding should be approached in a more scientific way, using materials and methods in the most appropriate way.

It should be understood that the method and designs illustrated represent only one idea which however would be adaptable and would serve many of the points made above.

### Constructional considerations affecting planning

**WALLS:** Fullst advantage taken of load bearing walls with large openings confined to non-load bearing panels. **FOUNDATIONS:** Deep foundations confined to load bearing walls only and of simple form. **BRIDGING:** Floor and roof members supported on main walls only. 16 ft. 6 in. has been found the limit of economical span for members 9 in. by 2 in. These have been chosen as being of a dimension fitting in with the general dimensioning. **STAIRS:** Straight flights, parallel to span to avoid trimming and also pointloads on trimming joists. **OPENINGS:** Large span openings to

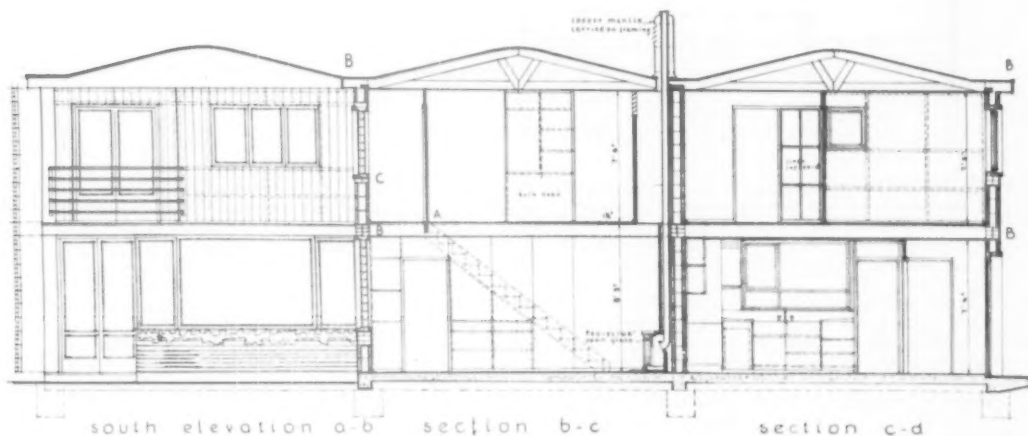
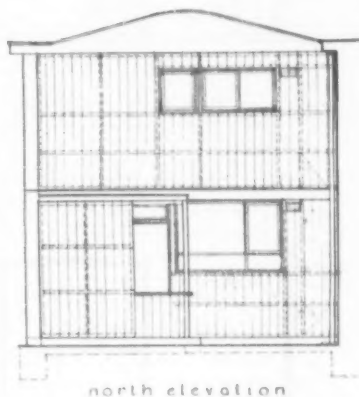
ISOMETRIC  
VIEW



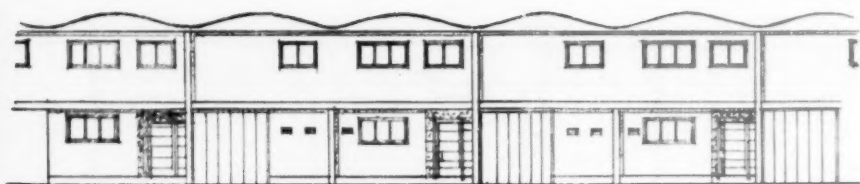
coincide with span. DIMENSIONS: All multiples of  $4\frac{1}{2}$  in. on plan and 3 in. in heights. HEIGHTS: Kept to permissible minimum due to open joist treatment.

#### Construction and materials

**WALLS:** Load bearing walls of 9 in. by 9 in. by 18 in. foamed slag blocks, rendered externally. Could be replaced by 9 in. bricks, which would have to be lined to give same thermal insulation. **PANEL WALLS:** Shown here in timber frame, shop fabricated, lined internally with woodwool slabs. Could be lightweight hollow blocks suitably lined. **PARTITIONS:** Woodwool cement slabs, built off and pinned to joists bonded in 1:1 gypsum mortar for quick drying and no shrinkage. **OPENINGS IN LOAD BEARING WALLS:** Where unavoidable precast concrete or reconstructed stone frames are built up in four parts, the lugs acting as weather bars. No further lintols required. **Frames**



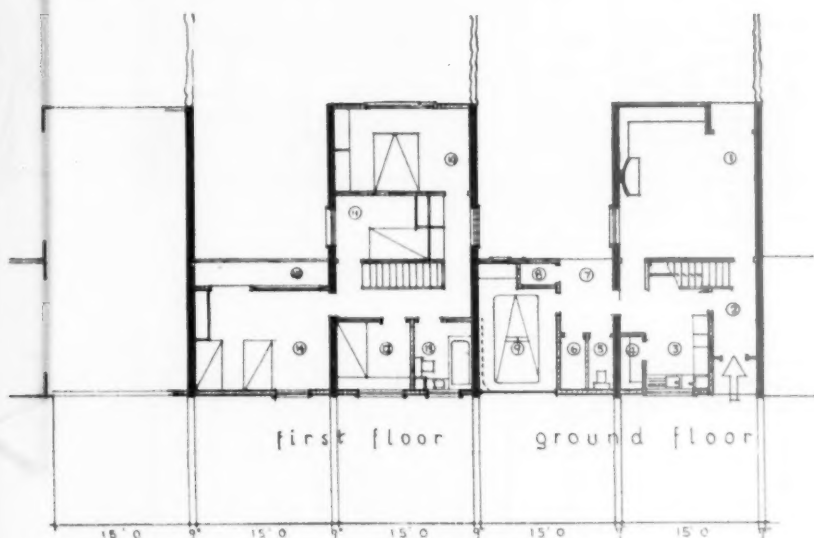
SINGLE SPAN HOUSE BY H. W. ROSENTHAL



north elevation



south elevation



plans

scale : 16 ft. to an inch.

- ① living room
- ② hall
- ③ working kitchen
- ④ larder
- ⑤ w.c.
- ⑥ fuel
- ⑦ covered yard
- ⑧ drying cabinet
- ⑨ garage
- ⑩ bed room 1
- ⑪ bed room 2
- ⑫ bed room 3
- ⑬ bath & w.c.
- ⑭ bed room 4 & play room
- ⑮ balcony

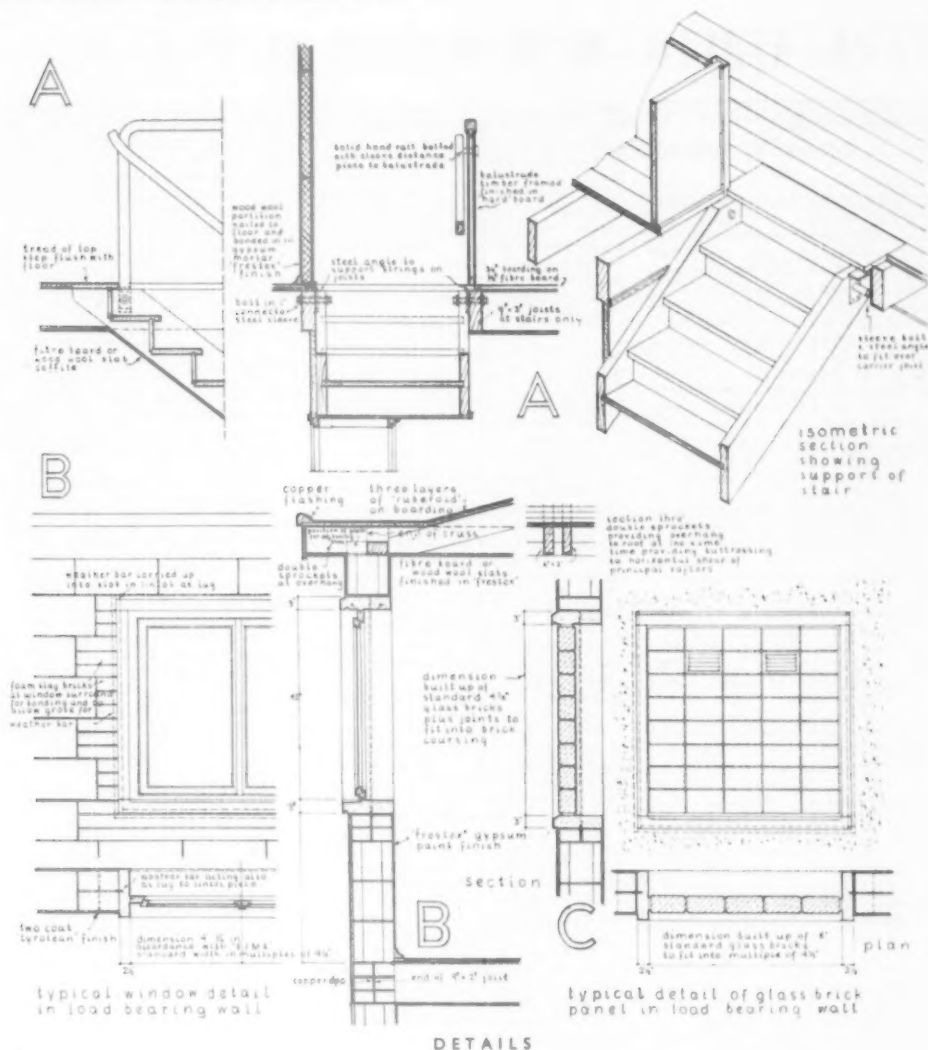


## ALTERNATIVE TYPE

also act as templates to bricklayer. FLOORS: Timber joists 9 in. by 2 in. at 18 in. centres, strengthened under partitions and stairs. Fibreboards nailed to tops of joists and t. and g. floor boards to fibreboards. Thus

necessary rigidity of floor boards is retained over wider span and good sound insulation is provided. The open ceiling presents a neat underside. GROUND FLOOR: Bitumastic cement floor or rubber latex on





DETAILS

concrete. Avoids need for further D.P.C. membrane. FOUNDATIONS: Concrete piles made by simple hand auger, linked with situ R.C. beams. ROOF: Specially shaped light trusses, boarded, underside woodwool slabs, resting on continuous wall plates. Covered by roofing felt. STAIRS: Simple closed spring type, fixed by bolts and cleats to flanking joists. DOORS: Flush in standard pressed steel surrounds, the latter acting as stiffeners to light partitions. WINDOWS: Wood, based on E.J.M.A. standards with dimensions of multiples of 4½ in. on plan and 3 in. in height.

Although timber panel walls have been suggested the total quantity of timber used in one house would not

exceed 1.45 Std. If the panel walls were replaced by lightweight hollow tiles and the floors and roof boarded by prefabricated stressed skin plywood such as "Jig-wood" the timber would be reduced to .97 Std.

All questions of stability are based on careful calculations.

Generally speaking, houses designed on these principles would give a performance in many ways superior to those built in a more conventional manner without reduction in amenities but considerable saving in overall cost.

Total floor area, 1,150 ft. super. No further out-houses are required.

# LIBRARY NOTES

## Switzerland Builds

By G. E. Kidder-Smith. Price 42s. net.

KIDDER-SMITH'S new book "Switzerland Builds—its native and modern architecture", published by the Architectural Press, London, and Albert Bonnier, New York and Stockholm, is a worthy successor to "Brazil Builds" by the same author. The book begins with an investigation of the traditional building technique of each Canton, drawing special attention to the aesthetic and structural use of materials. It is interesting to note how many of the contemporary buildings use structural details directly descended from their traditional counterparts. This first section of the book takes each Canton separately and briefly describes its historic and economic background, and with the aid of superb photographs illustrates the architectural richness of the vernacular building, which as Siegfried Giedion says in his historic introduction, "is still the architectural backbone of the country".

The portion of the book dealing with contemporary buildings is arranged in sections, each dealing with a particular building type, housing, churches, schools, hospitals, etc., again illustrated by photographs taken specially by Kidder-Smith. These photographs are not pictures taken when the buildings were first erected, but recent photographs which show how the careful detailing and superb craftsmanship of Swiss contemporary building results in an enhanced appearance after years of normal use. For example, the Doldertal Flats at Zurich (1936), by Alfred and Emil Roth look even better to-day than when they were first built, more than 13 years ago. How few contemporary buildings in this country have successfully withstood the relentless attacks of weather and their users.

One of the most fascinating chapters of the book is the one devoted to contemporary Swiss churches, which are outstanding for their high standard of design. Switzerland is probably the only country in the world today, where traditional forms are practically unknown for new churches. Werner Mosers' Reformed Protestant Church at Altstetten built in 1941 is probably still the most beautiful contemporary church in the world, it combines simplicity and dignity, and could well stand as an example to all in England concerned with the future of Church building in this country.

There are so many good things in this beautifully produced volume that it is difficult to select isolated examples for particular mention, the only solution to the problem being the recommendation that every architect and student interested in contemporary architecture should acquire a copy of this book.

The typography and layout of the book are excellent, and if any criticism can be made it concerns the plans accompanying the illustrations of modern buildings. Many of these plans are inadequately lettered, and some have room descriptions in English, some in

German and some in French. It would have been more helpful if all descriptive matter on the plans had been printed in English in conformity with the general text.

"Switzerland Builds" was produced as a result of a Guggenheim Fellowship awarded to Kidder-Smith in 1946, and a companion volume "Sweden Builds" has also been produced. We look forward with eager anticipation to "Sweden Builds", and perhaps one day to "England Builds".

EDWARD D. MILLS.

## Trees for Town and Country

By S. R. Badmin and Brenda Colvin. Lund Humphries. 25s. Second edition, 1949.

A LIME-green dust-jacket instead of viridian, end-papers with Badmin's exquisite drawings reduced to a small scale to show the comparative sizes of the different trees and a new note by Brenda Colvin on Pruning and Trimming, these are the only differences which I can discover in the second edition of this famous book.

These are all slight improvements on what already seemed perfect before and this therefore set me trying idly to find other points which might be improved. Surprisingly enough, I found a number to suggest and here they are:

Why acknowledge politely "the use of photographs" and not the photographers themselves? All publications should give photographers their due now and not in a hundred years' time as seems to be the fashion.

Most of these photographs of trees are indeed hard to beat, but again I found a number of them not quite so good as one had remembered them.

The Acacia is not flattered by the tennis enclosure at its base, nor is its characteristic feathery foliage caught as in the Ailanthus picture. Then it was obviously tempting to skip the almond blossom but really this is its main beauty and I have always found the leaves full of that swollen horror, the peach leaf blight, for the rest of the summer. The Ash does not here display its very marked branching habit, the ends turned up like the fingers of waiting hands and its thin leaves always revealing the branches even in summer.

The Holly, one of our loveliest native trees here lacks its dark glitter and its berries, and becomes a shapeless lump. Lumps in the photographs too are the Lime, Sycamore and Walnut, all evocative names, but their atmosphere is not evoked except in the drawings.

Lastly the Willows, surely a gift for a sensitive photographer, are a disappointment, one weeping, not by the waters of Babylon but in a balustraded enclosure and the White Willow, not white at all, against may be, a luminous Norfolk marsh and sky, but blackish and growing in a dull park.

The sixty trees named in the book are well chosen and obviously this choice was limited to landscape rather than garden trees and so spotting omissions

is probably all too easy. Here are some of my own favourites missed: *Betula dalecarlica*, the cut-leaved silver birch of marvellous carriage and foliage; *Prunus subhirtella autumnalis*, the winter-flowering cherry, which in December glimmers against a yew in my own garden and are then more appreciated than all the two orchards in spring; *Rhus typhina*, the little sumach with its tropical looking leaves beloved of modern Swiss architects and *Cotoneaster frigida*, both good and distinctive, though only small trees.

My last requirement for a future edition is for a special list of the trees grouped under their favourite soils. These are stated in the text but would be better classified on one page which would save many keen but misguided amateur planters from common errors of choice.

For the rest, this book lacks only sufficient praise. Its brief lucid text, admirably to the point, its fine classical photographs, its drawings which no modern student can now afford to leave off his facades (or does) and the whole appearance and layout still amount to, well, near-perfection.

But faster than such good books full of wisdom can appear and far faster than trees can grow, trees are being felled in hedgerows, parks and woods everywhere and they are not being replaced. Here is a free harvest and everyone is pleased and gets a quick easy profit, the poorish owner, the timber feller, the desperate joinery works.

At the bottom of my orchard stood a line of great Ashes, over a hundred years old. They were not mine and are being cut down as I write, sold for £3 10s. each. They filled the huge Norfolk sky and kept off the prevailing winds, throttling them in their branches high and low. Their loss and the winter winds will be almost unbearable except for one thing. I shall start up some willows on my own land, the Crack, Bat, Red, Yellow, White, Grey, Blue, Violet and one to weep for the dead Ashes. But how often is there this consolation made?

HERBERT TAYLER.

## BOOKS RECEIVED

*Switzerland*, by John Russell. Published by Batsford. Price 15s.

*The Face of Wales*, by Tudor Edwards. Published by Batsford. Price 12s. 6d.

*A History of Architecture*, by H. Heathcote Statham. Published by Batsford. Price 21s.

*The English Cathedrals*, by Herbert Felton and John Harvey. Published by Batsford. Price 18s.

*British Standards for Water Fittings*. B.S. Handbook No. 12, 1950. Published by the British Standards Institution, 24-28 Victoria Street, S.W.1. Price £1 5s.

*The Mercantile Year Book and Directory of Exporters*. Published by Lindley-Jones & Bros. Ltd., 52 Bishopsgate, E.C.2. Price £1 6s.

*Romanesque Architecture in England*, by Sir Alfred Clapham. Published by Longmans Green & Co. Ltd. Price 2s.

# NEWS of the BUILDING INDUSTRY

## THE EFFECT OF NATIONALIZATION ON THE BUILDING INDUSTRY

The Working Party Report observed that "the existing layout of building concerns provides the necessary flexibility and variety of capacities required by building operations." The Working Party recommended no alteration whatever over the present layout.

Reference to this Report brings me to a feature connected with the probable reason for the selection of this subject for consideration. I presume that you have chosen it as being a matter now before the public as the result of the adoption, by a recent conference at Ayr, of the National Federation of Building Trades Operatives, of proposals made by their executives for the nationalization of the Building Industry. These have been made although neither the Government nor the Labour Party has ever made any suggestion in their programmes for including building amongst the industries considered for nationalization.

The operatives held a conference last year at Porthcawl when they instructed their executives to prepare and submit proposals for nationalization to the 1950 Conference. In the meantime, however, certain important things happened. The first was the publication of the Working Party's Report. The Working Party consisted, in its personnel, of representatives of all sides of the Industry, including the employers, the operatives, the specialists and sub-contractors, and the professions. The operatives' representatives were Sir Luke Fawcett, the President, and Mr. Richard Coppock, the secretary of the National Federation of Building Trades Operatives. That Report, which was unanimous and with no minority qualification, expressed itself, as I have said, as being satisfied with the existing layout of the Industry. It did not even mention the word nationalization, and it deprecated any idea of a Development Council for the Industry.

About the same time there was also published the Report of the Building Industries Productivity Team on which the operatives were represented. That Report found that the make-up of the Industry, in regard to size of firms and operatives employed, was very similar in the States to that in this country, and some stress was laid upon the efficiency of the American construction industry. Now suddenly, in spite of all this, we have a recommendation submitted to the operatives' conference at Ayr going all out for nationalization of the Industry in this country, and amongst its principal signatories are the President and the General Secretary of the Operatives' Federation, who, only a few months before, had signed the Working Party's Report. This position does not seem to us to make any sense and it perhaps accounts for the extraordinary nature of the proposals made to the Ayr conference.

*The accompanying article is condensed from an address to the Hackney and District Chamber of Commerce by G. H. A. Hughes, F.R.I.C.S., F.I.Arb., F.R.S.A.*

*Mr. Hughes opened by quoting some questions put by Mr. J. R. W. Alexander, vice-president of the Institute of Arbitrators in an address on Nationalization and the Professions. These were*

- 1. If monopolies are contrary to the public interest, why are they less so when established by the State?*
- 2. Is Nationalization the only means of achieving increased output, economy, service and lower prices?*
- 3. What grounds are there for concluding that industrial relations will be better under Nationalization than Private Enterprise?*
- 4. Can the worker combine against and withhold his labour from the State, and is it the Socialist conception that he should be permitted to do so?*
- 5. Is it the irony of fate that the men who are worst hit by Nationalization are the great Trade Unions and their leaders?*
- 6. The field from which competent Executives can be chosen being none too wide, does the State expect to place itself in a better position than private enterprise in this respect?*
- 7. Will the domestic and industrial consumer continue to enjoy freedom of choice?*
- 8. Will taxation received by the Exchequer from a Nationalized Industry be less than it would have been had the component units been separately assessed as under private enterprise?*

The whole of the operatives' document and proposals are based on the assumption that it is inefficient to have 125,000 firms in the Industry, and without accepting that assumption there would be no proposals to follow. In the light of that fundamental assumption it is particularly remarkable to note that the primary proposal is to nationalize only those firms employing more than 20 men. This would mean taking over some 9,000 firms covering some 670,000 operatives, and would leave free from nationalization some 115,000 firms employing nearly 300,000 men.

Thus, from the very start, the scheme would do practically nothing to achieve the reduction in the number of firms which the operatives regard as justifying nationalization. This aspect has been very carefully examined. It seems to leave only one possible conclusion, namely, that it would be either impossible, or undesirable, to nationalize work which is generally

that of repair and maintenance, for that, in the main, is what these firms and those operatives are engaged upon.

On further examination of the operatives' proposals, however, this cannot be the reason. They go on, later in the Report, to suggest, without explaining how, that the general wages and conditions of the nationalized section of the Industry would so far, and so rapidly, improve that before very long the 300,000 excluded operatives would find their way into the nationalized service. If this were so, then it must mean that the country will be able to do without men for repair and maintenance and smaller works. But will it in fact be so? Has the history of the nationalized industries so far shown any marked improvement in the satisfaction of the operatives in their wages and conditions of service? Has there been closer co-operation and mutual understanding between employer and employed?

*(Continued on page 210)*

## FITTINGS DOOR FURNITURE

C3/4

Made from single pieces of solid block Perspex and specially treated these door, drawer and shutter knobs have the appearance of crystal. It is claimed that they will not chip or break. The base of the knob is in chromium. This furniture is sold by the set which comprises one pair of handles, one pair of chromium key plates, one pair of Perspex escutcheons and a spindle. There are various shapes and designs. All are obtainable from certain large stores or through the normal trade channels.

## SERVICES FLUORESCENT (STREET) LIGHTING

B1/2

Shown for the first time at this year's British Industries Fair the unit illustrated is a fluorescent street lighting fitting for side lighting. The fitting houses two 2 ft 40 watt fluorescent lamps. The photograph shows the fitting in its normal mounting position.

## SERVICES ELECTRIC WIRING, ETC.

B5/1

A flush fitting electricity consumers' supply control unit measuring 2' 6" x 15" x 7½" (approx.). The unit weighs 40 lb. and is designed to contain all electrical meters, fuseboxes, etc., for domestic installation. The finish is in cream stove enamel. The illustrations show the construction of the box and its external appearance when built into a wall at a convenient height for easy access. The unit was first produced in August, 1949. The design is covered by B.S.1454.1948, Appendix B. The unit is available on home and export markets.

## PLANT TOOLS

E3/2

This portable motor driven hacksaw weighs 89 lb., is 36" long x 12" wide x 20" high and takes a 12" blade. The swivel vice, adjustable to 45°, takes metal of 4½" x 4½". Saw stroke is adjustable from 2" to 4". The motor is ½ H.P. with self oil bearings.

THE ARCHITECT AND BUILDING NEWS,  
AUGUST 18, 1950

## MOSAICS

The names and addresses of manufacturers of any item illustrated in MOSAICS, together with more detailed information relating to their products—including price and availability—will be forwarded to readers on request.

Letters should quote the serial number and be addressed to:

The Associate Editor,  
The Architect and Building News,  
Dorset House,  
Stamford Street, S.E.1.

Please mark the envelope MOSAICS.

## INFORMATION

### AND

## CATALOGUES RECEIVED

- Air-Maze, Ltd., is now the correct designation of the firm which was Air-Maze (Great Britain), Ltd.
- The Franco-British Electrical Co., Ltd. have issued a pamphlet giving types, dimensions and unit consumption of their fluorescent lighting fittings equipped with "Luxorlite" light diffusing system.
- A new publication, Addendum No. 3 (PD.1023) to B.S. Handbook No. 3: 1947, has been issued to bring the 1947 edition and Addendum No. 2 (PD.905) up to date. The new addendum covers summaries of 14 additional British Standards, six revisions of British Standards summarized in Handbook No. 3 and 12 amendments to the summaries in Handbook No. 3 and Addendum No. 2.
- "Precautions against Fire and Explosion in Underground Car Parks," No. 28 in the Ministry of Works' Post-War Building Studies series, newly published (H.M.S.O., 1s.), is a technical review of interest to authorities concerned with the formulation of safety regulations, to architects, structural and ventilating engineers, as well as to makers of fire-fighting equipment. The owner of underground car parks will also find useful hints.
- "Recommendations for the Design and Fabrication of Arc Welded Structural Steelwork, Built-up Girders and Compression Members" is a memorandum published by the British Welding Research Association, 29 Park Crescent, London, W.1.  
The memorandum contains advice on the general arrangement of built-up members with details of the construction and the welding required to ensure satisfactory behaviour under load.  
Compression members are dealt with in three groups: A, members built-up mainly from plates; B, members composed of rolled sections spaced and connected by laticing or battening; C, members built up from rolled sections welded together. Copies of the memorandum can be obtained, price 1s. 6d., together with the first in this series, "Stanchion Bases, Caps and Joints," price 9d.

## \* INTEREST \*

## TOO TRUE TO BE GOOD

THIS MONTH'S SELECTION, which we make no apology for lifting from the L.M.B.A. Directors' letter to members, tells of the contractor who asked a Government Department for permission to acquire and export three 30-cwt hoists, complete with electric motors and starters, voltage 400/440, 3 phase 50 cycles.

The reply granted permission stating: "You may therefore proceed to place your order for such hoists, complete with electric motors and starters. It is regretted, however, that permission cannot be granted for you to acquire for export the 50 cycles."

THE BUILDING CRAFTS TRAINING SCHOOL, conducted by The Worshipful Company of Carpenters, will reopen on Monday, September 11th, 1950, for Day Classes, and Monday, September 18th, for Evening Classes for Apprentices and others engaged in the Building Crafts in Carpentry and Joinery, Brickwork, Plumbing and Welding, Painting and Decorating.

All inquiries concerning the classes should be addressed to the Director at the School, or to the Clerk to the Worshipful Company of Carpenters, 28 Austin Friars, E.C.2.

A NEW COMPRESSOR has been produced by the Hymatic Engineering Co., Ltd. Designed to provide air completely free from oil for many purposes including pest control spraying, and spray painting, etc., this compressor is said to require no lubrication whatever and very little maintenance.

The compressor is built into a special electric motor having a rigid shaft with main bearings of generous capacity which are grease packed for long life. The compressor, crankcase and cylinder head are of aluminium alloy—the head being finned for effective cooling.

Two models are being produced. Model DP 6 consists of the compressor mounted on a 1/6-h.p. motor while model DP 7 is fitted with a small air receiver in addition. The dimensions are: Model DP 6 12½ in long, by 10½ in high, by 9½ in wide. Model DP 7 is 18½ in long, 11½ in high and 9½ in wide.

THE TASMANIAN GOVERNMENT is to send a building contractor to the United Kingdom to examine home building. His chief job while in England will be to induce building tradesmen to migrate to Tasmania. He will also inspect Britain's latest prefabricated house and look into the possibility of buying building materials for Tasmania.

WARERITE LIMITED have taken the opportunity presented by the Brewers' Exhibition—Olympia, October 2nd-6th—to introduce Warerite ceiling panels for the first time to the public. These consist of Warerite veneers laid on a special light-weight core which facilitates easy handling in erection. The examples on view will demonstrate three different surface textures—roughcast, fluted and pinpoint. Smooth-surfaced ceiling panels are also available.



Part of the Tummel-Garry Hydro-electric scheme in Scotland required the raising of the level of the River Tummel by 50 ft by means of a dam at Pitlochry. This in turn brought up the problem of a bridge to replace the old stone one at Clunie (near Pitlochry) which was to be demolished as it would be submerged in the reservoir. The answer is this bridge, 310 ft 6 in long overall with a centre span of 172 ft 6 in and two side spans each of 69 ft, made entirely of Duralumin.

The metal is Duralumin "H" used in the full heat-treated condition. All rivets were made in "MG5" light alloy and driven cold by ordinary percussive means. Time was saved by the lightness of the material—the engineers found there was no necessity to wait for a crane to lift even some of the largest parts. The structure will not be painted so as to indicate clearly the nature of the material used.

The Clunie Bridge was constructed and erected by P. & W. MacLellan Ltd., of Glasgow, to the design of Sir Alexander Gibb & Partners.



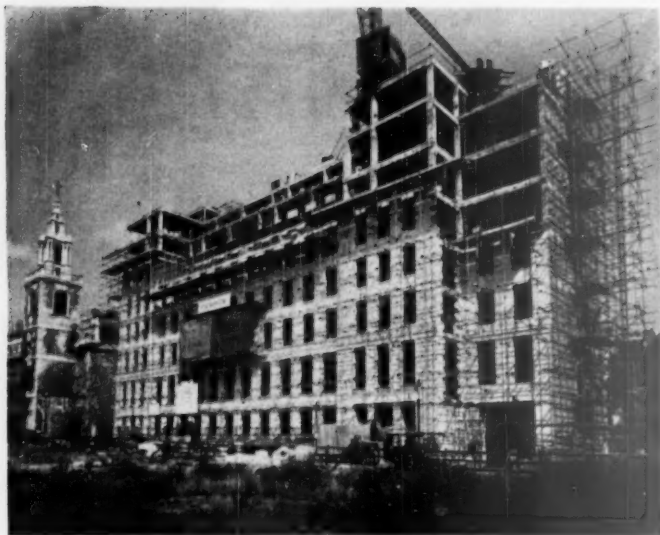
THE ASSOCIATION OF MASTER STONE MASONS is arranging the annual practical examination for London Masonry Apprentices. The Examination will be held this year on Saturday afternoon, September 16th, at 3 p.m., at the works of Messrs. Webber & Corben, Ltd., Nelson's Row, High Street, Clapham, S.W.4, and it is expected that some 70 boys will be taking part.

The boys will be classified into five classes, according to the length of the time they have been at the trade, and they will be judged not only on the results of a two-hour practical test, but also on their

school report. Suitable prizes are to be awarded to those attaining the necessary standard of efficiency, and the prize distribution will be held at the offices of the London Master Builders' Association, 47 Bedford Square, W.C.1, on Tuesday, October 3rd, 1950, at 2.30 p.m.

It is proposed to give the boys taking part a trip to Bognor on Tuesday, September 19th. It was found in the years before the War that these annual examinations and outings proved very effective in stimulating a keener interest in the craft and also a competitive spirit between boys from different firms.





WHAT AND WHERE ?

A COMMITTEE OF THE L.M.B.A. have for some time been discussing the fact that lavatory basins seem so frequently to crack after a new basin has been installed. It was finally ascertained that the new British Standards Specification pro-

vides for a basin with a bevelled edge at the outlet instead of the old type of rebated outlet. This brought the metal of the old outlet in close contact with the basin itself and was largely the cause of the damage. The matter has been dis-

cussed with the Merchants at the Liaison Committee with B.I.D., and it was found that the new basins are coming out faster than the new type of outlet. It is therefore important to note that, where a builder is required to install a new lavatory basin, his representative should ascertain whether the outlet is of the right type and, if not, to order a new outlet and pipe with the basin. This would, of course, have to be allowed for in the quoted price. The Merchants have agreed to help in this matter, and will instruct their travellers and agents to note it carefully and also, it is hoped, they will be able to affix instructions to the basins.

DETAILS OF ALL BUILDING COURSES available at Technical Colleges and Schools in the London Region are given in full in a memorandum just published by the L.M.B.A. This publication should be of considerable value, not only to those firms employing apprentices, but to all members of the Building Industry in London.

Copies of this memorandum, which contains details of courses suitable for everyone from apprentice craftsmen to senior managerial and supervisory staff, are available free on application to the L.M.B.A., 47 Bedford Square, W.C.1.

POPULAR SCIENCE, the American monthly, for August, carries a description and photograph of a box mortar spreader which by the turn of a handle travels along a wall on small wheels while spreading mortar evenly astern. The inventor claims elimination of waste from droppings and uneven spreading.

## GOOD, BAD OR INDIFFERENT?

By A. FOREMAN

No. 7

### Plastering Insulation Board

When I go on different jobs and see insulated board being plastered I often think I should like to go back a year later and see whether my estimation of the probable results will turn out to be correct. Sometimes the fixing is the source of the trouble, but at other times the plastering is to blame. To get a finish which will look right and stay right certain precautions are necessary, particularly to avoid cracks forming at the places where the boards have been jointed.

These precautions are more easily understood if the likely causes of the cracking are first explained. Insulation boards have a fairly large "moisture movement," that is, they expand on becoming damp, either during the plastering or because of changes in the weather, and shrink again on drying. If the boards are fixed to a backing over the whole area, as when used as permanent shuttering, this movement will be "restrained," but in most jobs they are fixed to studs or grounds at a limited number of points, so that changes in moisture content lead to closing and opening of the joints at the gaps. As an alternative to fixing to a solid backing, this movement can be restrained by fixing reinforcement across the joints

to provide additional strength in the plaster at the critical points and also by using a plaster which will itself restrict the expansion and shrinkage of the boards.

The joints, which should have been left open by fixing the boards  $\frac{1}{4}$  in. apart, should be covered with strips of hessian scrim, 3 to 4 in. wide, applied over all the joints. Except with certain plasters (see below), metal scrim may be used as an alternative and this is often better, but it should be galvanized and any cut ends painted to prevent staining of the plaster by rust. Whichever type of scrim is used, it should be bedded and held in place by a layer of neat plaster laid on each joint before the main plastering is commenced. It may be rather easier to get the strips neatly bedded, particularly when metal scrim is used, if a few galvanized nails or staples are also used.

It is a little difficult to make a good joint between an insulation board and another backing, such as brickwork or old plaster, but it can be done if a strip of scrim 6 to 8 in. is used, well fixed on both sides of the joint. Generally, however, I think this is best left alone. A clean break is really safer and if an open groove is not acceptable it can be covered by a neat beading.

It is very important to use the right

plaster. There are now very many plasters on the market sold under proprietary names and not all these are suitable for use in contact with insulating board. What is wanted is a plaster that will set quickly without much expansion, form a strong bond with the board and after that check the expansion or shrinkage of the board. For convenience of reference, here is the classification given in B.S. 1191, "Gypsum and Anhydrite Building Plasters":

Class B—Retarded hemihydrate gypsum plaster.

Class C—Anhydrous gypsum plaster.

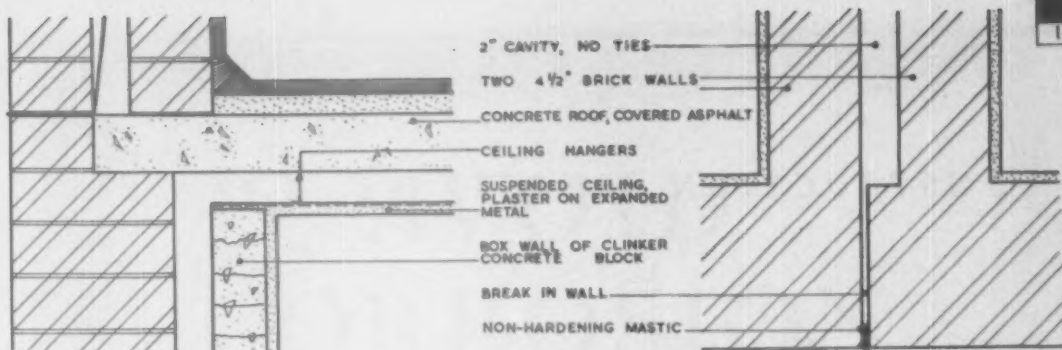
Class D—Keene's or Parian plaster.

Class E—Anhydrite plaster.

The first three classes are all available as undercoat, finishing or dual-purpose plasters, but Class E plaster is always a dual-purpose type.

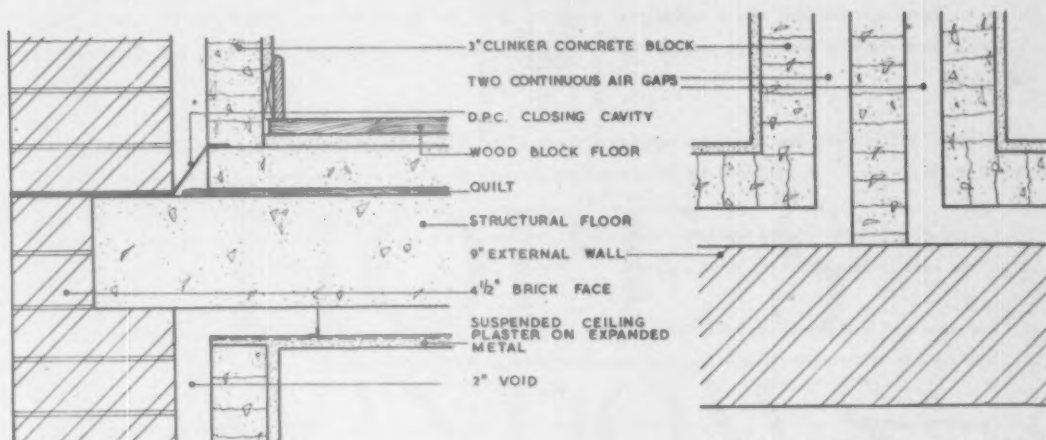
The position is complicated by the fact that not all plasters in these classes are suitable. If Class B plaster is used, you should insist on getting a "Board plaster" which will have a low expansion on setting to avoid forcing the boards out of true. Particular care is necessary with Class C and D plasters as only certain brands are suitable. Therefore, it is wise to get the manufacturer's assurance on this point and then to follow his instructions precisely. There is, I believe, only one brand





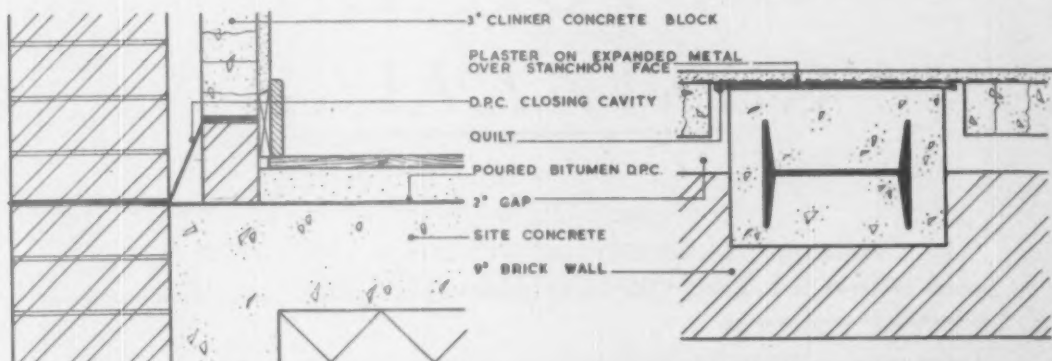
1 DETAIL AT ROOF LEVEL.

4 PLAN AT JUNCTION OF EXTERNAL WALL WITH PARTY WALL IN HOUSES.



2 DETAIL AT INTERMEDIATE FLOOR.

5 PLAN AT JUNCTION OF EXTERNAL WALL WITH PARTY WALL IN FLATS.



3 BASE OF WALL.

6 PLAN SHOWING METHOD OF WORKING ROUND STANCHION.

## CONSTRUCTION SHEET L.2, C.17.

### Editorial Notes

This sheet gives six details of external walls showing methods of sound insulation.

1. The clinker concrete box wall is constructed entirely free of the structural wall. Noise from one room is thus prevented from reaching another by way of the structure. The ceiling is suspended on hangers and the cavity is maintained.

2. A quilt, laid between the structural concrete floor and the screed above, isolates noise and prevents it being passed to the structure. A flexible d.p.c. is incorporated to close the cavity between inner and outer leaves.

3. A layer of cork or fibre board insulation is provided at the base of the box wall. A brick-on-edge plinth is provided to the box wall to take the damp proof course.

4. The party wall is of two 4½ in. brick skins, wall ties being omitted. The break is continued through the external wall and the gap is sealed with non-hardening mastic to prevent the entry of water.

5. The party-partition wall has a rigid central leaf with 2 in. air gaps on either side. The internal clinker concrete block box walls are not connected with this rigid leaf.

6. A method of working round a stanchion is shown, when it is desirable to keep the internal face flush. A quilt is laid between the stanchion face and plaster.

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We welcome comments from readers. These will be summarised and published. Letters should reach us as early as possible to avoid time lag.

AVOID  
CORROSION

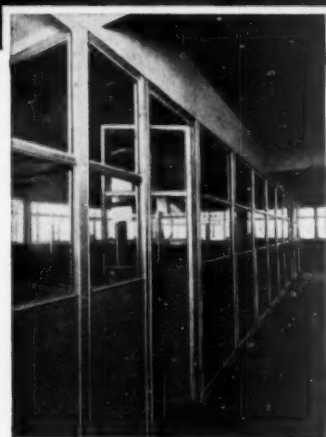
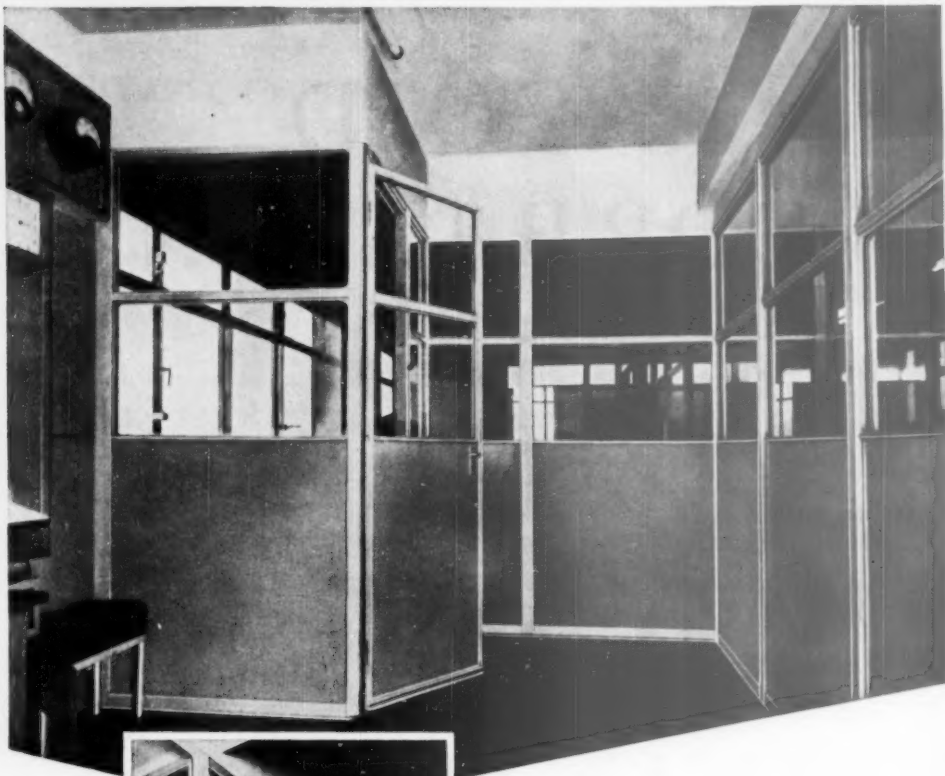


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of Class E plaster available, and this is quite suitable. A point to watch is that acid types of Keene's plaster and Class E plaster will attack metal scrim so that with these only hessian should be used.

The next question is to decide on the method to be used—single-coat or two-coat work? Single-coat plastering is usually preferred on account of its lower cost, but it requires extra care to make a good job. The plaster is so thin that the boards must be absolutely flat, the joints even and the scrim well bedded. On the other hand two-coat work, although in itself more expensive, provides more opportunities for covering up irregularities of this kind, some of which may not be

apparent until the undercoat has been finished.

For single-coat work, the plaster should be applied neat and, in general, lime should not be added. It should not be less than  $\frac{1}{4}$  in thick as very thin skim coats are liable to dry too quickly and have not sufficient strength to restrain the boards.

For two-coat work, the main precautions are concerned with the undercoat as it is this which is in contact with the boards and which, on account of its greater thickness, provides most of the restraint. For this undercoat it is usual to mix sand with the plaster but not more than 1½ parts (by volume) of sand should

be added to one part of plaster. It is important that the sand should be clean and sharp—Class A of B.S. 1198 "Sands for Plastering," covers suitable sands, but if it is necessary to use the finer sands covered by Class B, the amount used should not exceed one part of sand to one of plaster. Here, again, lime must not be added. The undercoat should in no case be less than  $\frac{1}{4}$  in thick, and should be allowed to harden before the finishing coat is applied.

For the finishing coat, any type of plaster may be used (a further advantage of two-coat work), and lime may be added, because this coat has little or no effect on the insulation boards.

## DOMESTIC ELECTRIC WIRING—No. 2

### Some Modern Tendencies

By T. C. Gilbert, M.I.E.E.

#### (2) Earthing Difficulties

The satisfactory earthing of metal-clad wiring installations and equipment connected thereto has usually been by a connection to the incoming water main supply pipe. For many years this has been standard practice. The principle is, that with any leakage of electricity to the steel conduits or the metal casings of equipment the fault current will flow harmlessly to earth, and with the blowing of the phase fuse the current will be cut off to the affected circuit or apparatus. This has postulated a very low resistance and heavy current carrying connection with earth, and the water system, with its wide underground ramifications through miles of metal main pipes, ensures the necessary intimate contact with the mass of the earth.

This arrangement has to some extent been upset in recent years, owing to the growing practice by the water authorities of using cement-asbestos main pipe in place of iron. The service pipe itself is not satisfactory as an earthing medium, as it is usually of high resistance and is too short to make good contact with earth. Matters came to a head recently when an investigation by the Electrical Research Association into troubles encountered in Manchester and Rochdale, where the use of cement-asbestos main piping has proceeded to the extent that earthing to water pipes in houses was not only useless from the safety angle, but, on the other hand, set up enhanced dangers. The recommendations of the Association were to the effect that where cement-asbestos piping is used by the water supply authorities earthing to water pipe services must be abandoned in favour of earthing to the lead sheathing of the electricity service cables.

This is not entirely satisfactory, however, and in fact many electricity supply authorities do not welcome earthing connections to cable sheaths—some definitely forbid it. It cannot always be relied upon that the resistance of cable networks is low, as much of the cable may be run in stoneware ducts, and cables often have a hessian or other covering, all of which prevents the close connection with earth that has previously been afforded by the water system. It is usually fatal to good safety by earthing if the resistance of the

earthing medium to earth is in excess of 1 ohm, and far too often the cable sheathing is well above this figure.

The position has been even more seriously complicated of late with the introduction of polythene and other plastic material service pipes into buildings. If this practice increases it may mean a complete absence of metal water piping in a building at all. That might be a good thing; at least no doubtful earthing connection to a metal service pipe in eventual connection with a cement-asbestos main could be made, with all the danger that this might introduce. As polythene-insulated electricity service cable, without lead sheathing, is now being introduced also, in the near future installations may be left with no earthing medium in the building at all. Earth plates or spikes cannot be contemplated with the present loadings associated with domestic installations, often amounting to 10 or 12 kW.

Unless an earth fault can be cleared quickly and with certainty, danger immediately arises by reason of the extension of a leakage voltage on to all metal connected with the doubtful earthing medium, and this includes the metal conduit system and any equipment connected thereto. The only possible answer is to eliminate as much of this metal as possible, as the existence of this leakage voltage gives rise to continuous risk of shock and fire. As regards the wiring system, elimination of metal can be secured with the employment of all-insulated methods, and there are three systems that can be employed—non-metallic conduits, tough-rubber-sheathed cables, or their modern counterparts, P.V.C.- or P.C.P.-sheathed cables. As stated in No. 1 of this series, the preference of the writer is for non-metallic conduits, especially of the flexible variety, and further reasons for this will be later developed.

The use of insulated wiring systems eliminates much of the metal incorporated in the average electrical installation, but there still remains that associated with the casing of equipment—motors, radiators, kettles, cookers, etc. Much electrical apparatus is now manufactured with plastic outer casings, which requires no earthing, and the recent 12th Edition of the Wiring Regulations of the Institution

of Electrical Engineers carries the all-insulated idea further by stating, in Reg. 1001 (B), that the metalwork of any portable appliance having double insulation conforming with the requirements of a British Standard is exempted from the general requirement for earthing. Such a British Standard, No. 1465, has already been approved for vacuum cleaners, and others will follow.

Where cement-asbestos water mains are used, or are at all likely to be used in the future, earthing to water service pipes must cease, and the all-insulated idea must receive fuller consideration. With earthing to such useless media not only are dangerous leakage potentials extended on to all conduit work, if of metal, but also to all water piping in the building. Worse still, where gas water heaters or geysers are used, the connection of the water and gas pipes to the same equipment means that the leakage will also be passed to the gas piping and mains, with serious risk of fire. The case is made out, therefore, for a wider use of all-insulated wiring systems, in connection with which this serious leakage menace cannot exist, apart from other advantages associated with this type of electric wiring.

Whereas in the past the best practice has been to use metal-clad installations, with earthing for safety, the present-day tendency is towards the elimination of as much metal as possible from the installation. It was once considered that steel tubes offered the best protection from fire; it is now realized that unless effective earthing for steel conduits can be ensured, and maintained throughout the life of the installation, then enhanced risk of fire may arise. As any water supply system may in the future contain cement-asbestos piping, when renewals or extensions are made, it is clear that complete insulation is the only answer.

The report issued by the Electrical Research Association in connection with the troubles encountered in Manchester and Rochdale is entitled "Electrical Resistance of Water Mains, with Particular Reference to the Asbestos-Cement Type," Reference V/T101, and this contains much of interest to those entrusted with the preparations of specifications for domestic electric wiring.



(Continued from page 205)

Has there been any greater freedom from industrial disputes, or has, in fact, the reverse applied? It might well be that the drift would prove to be the other way and we should then be left with a vast nationalized machine, with a rapidly diminishing number of operatives employed in it.

In an age of full employment we sometimes tend to forget that full employment means less mobility of labour, for labour will not normally travel distances to work if it can obtain work nearer home. The Building Industry depends in large measure upon a considerable degree of mobility of labour and it would be quite impossible for a nationalized Building Industry to keep itself efficient and its jobs staffed without such considerable mobility of labour. Would, therefore, direction of labour have to be the ultimate answer?

The family and team spirit—a remarkably essential feature of the Building Industry—combined with the excellence of the joint industrial machinery, has left the Industry with a record of being far ahead of many of the other major male-employing industries, in freedom from industrial disputes. You must go back well over 25 years before you find any record of a major upheaval by way of strikes in the Building Trade and, in order to avoid quoting further figures, I refer you to any issue of the Ministry of Labour Gazette where you will find my statement fully proved. Thus you have an atmosphere built up within the Industry which must be of inestimable value to every man or woman who wants any building work done. The facts in other industries so far do not seem to indicate that such an atmosphere could be maintained if nationalization were introduced.

If this scheme be thought impracticable, the operatives' alternative is to make the State the only party with whom a building firm can contract for work. Unless, of course, you leave out all your maintenance and repair work, which makes nonsense of the whole thing, it would accordingly mean under this second proposal that if anyone wanted a tap re-washed, or a sash-cord renewed, or any of the little services for which you so often call upon the jobbing builder, the builder could only do his work at the order of a Government department. The proposal is not worked out in detail. But, from experience, one can only imagine that a form, or probably several forms, would have to be filled up. Whilst the ponderous machine of bureaucracy was carrying out all the paraphernalia of procedure, that has become, alas, so familiar to us, before your builder could be instructed, your tap would go on leaking and your window would go on letting in the weather.

One had thought that the operatives had set their faces against monopolies. Yet here they are, in the matter which affects not only their livelihood as

workers but their own personal affairs and accommodation too, suggesting the setting up of an even greater monopoly. It used to be said that, when you ordered a Ford car, you could have any colour you liked, provided you asked for black, and it is not difficult to see something of that atmosphere in your own building work of the future if you allow the Industry to be nationalized. It is not in any way decrying the excellent work which Civil Servants in their various grades carry out, when I say that a machine of that size is too vast for the conduct of such personal affairs. In such a scheme there would have to be a very extended amount of decentralization, in fact the operatives' proposals specify this. As your State machine becomes larger and more decentralized, so you must have more and more worthy individuals with responsibility but without authority. Therein at once lies the trouble as we well know to-day and the wheels of industry become clogged with the grit of those who, anxious as they may be personally to help, can only say "Thou shalt not" until some higher authority can give the necessary sanction. Do we not at present suffer from a surfeit of those holding responsibility who can only say "Thou shalt not," whereas we need more of those in authority who can and will say that we can get on with the job?

Even as far as I have gone, however, is not really the end of the nationalization story. The operatives' proposals state quite categorically that a prerequisite for nationalizing the Industry is the nationalization of building materials and all merchandising. As the operatives include merchandising in this category, it is not only the production of those articles, but the merchants and the shops who handle them as well, which would have to come into the scheme. And so, presumably, a large part of your Woolworths, as well as your little local ironmonger, would all disappear into the nationalized concern, and if you wanted a cup and jug hook, or a picture hanger, I should have to "refer you to the appropriate department." It is not without significance that according to the proposals, the Co-operative Societies are to be excluded from nationalization.

Even there, however, we are not quite at the end of the story, because the last paragraph of the scheme says, "We think it right to say that we are not too happy about nationalizing the Building Industry while leaving the land in private hands." Thus it seems the Building Industry has a little time yet before it faces its fate because, first of all the land must be nationalized, next, the production of all materials concerned in building and the means of their distribution, and the Industry proper seems to come a somewhat unhappy third.

It is perhaps not without significance that, within a very few days of the adoption of these proposals by the Ayr Conference, the Minister of Works announced in the House of Commons that he proposed to disband the Mobile Labour Force, which so far is the one bit of nationalized Building Industry that has existed.

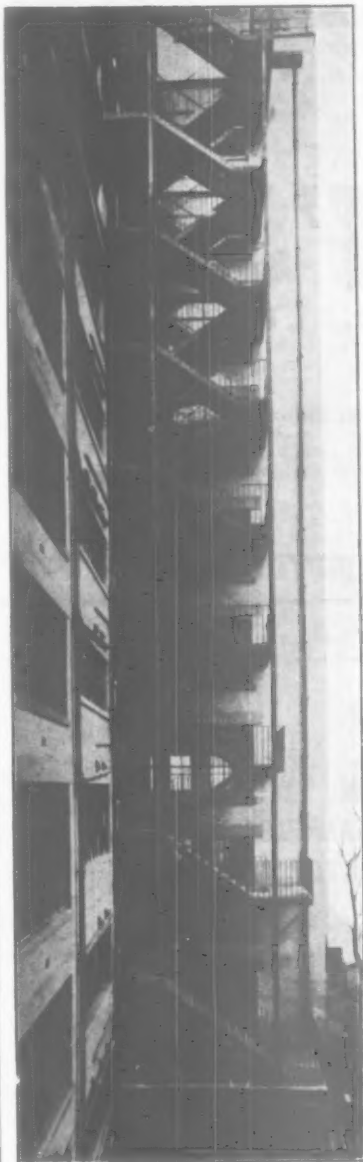
One more short word I must add upon the publicly expressed views of the operatives. Their main conference was followed by a conference of the A.U.B.T.W. Nationalization was again discussed and Sir Luke Fawcett is reported by the *Daily Herald* as having said that the Building Industry would be amongst the easiest to bring under public ownership. There was no reason to consider compensation, he said—all the builder had was his plant, which he sold or left idle if his tenants failed. In other words, deprive a man of his means of earning his living, and he and his goods and chattels are forced into the hands of the State. I wonder what the unions would have said if an employer had made such a proposal about his operatives! One can only hope that this comment either slipped out inadvertently or has been mis-reported, for anything farther from the British way of life and code of moral conduct it would be hard to imagine.

The remarkable thing is that all this has been and is being done and this position exists in an Industry which is probably more individualistic in its component parts than any other, and in which competition constitutes a real joy of existence. Builders will compete in the fiercest manner against each other for a particular job or contract, and will come straight from it, flushed with victory, or humbled by defeat, to sit alongside their competitor in a committee and to do a grand job of work in the interests of the welfare of the Industry, whether it be for the betterment of the wages and conditions of its employees, encouragement and training of apprentices, better safety precautions, research, reduction of building costs and education of all within its ranks. I find it difficult to believe that this atmosphere and spirit of service and co-operation could be maintained if the Industry, with its 125,000 firms and million and more of operatives, were to be brought under the operations of a vast State machine.

The Building News section of The Architect and Building News for September 1st and September 8th will be devoted to special features. News of the Building Industry, Mosaics and the serial articles will be discontinued for these two weeks and will begin again with the issue of September 15th.



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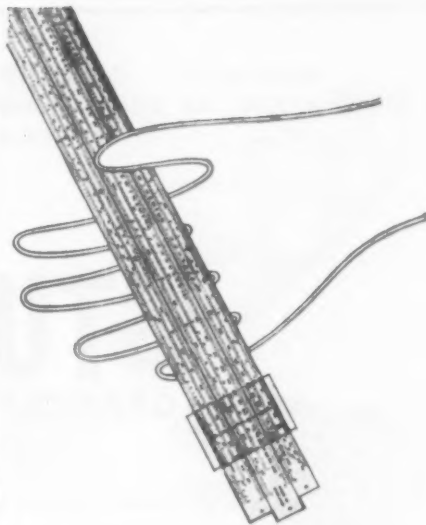
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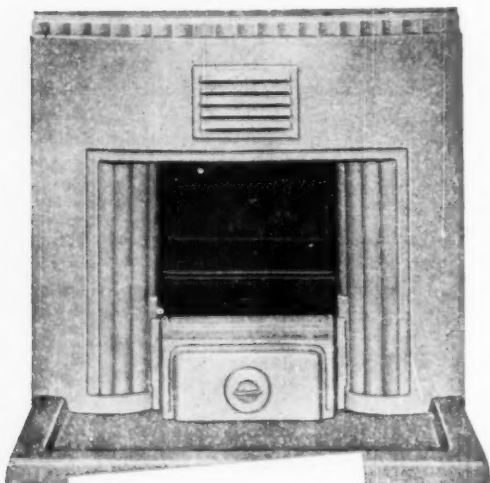
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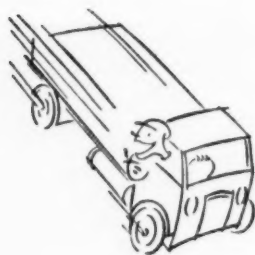
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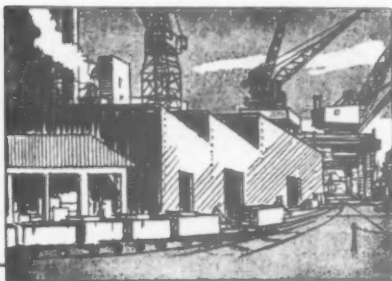
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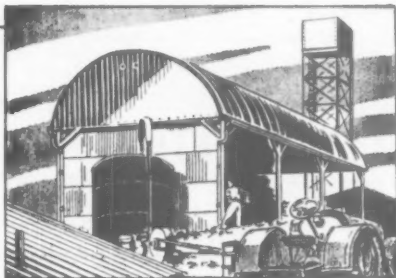
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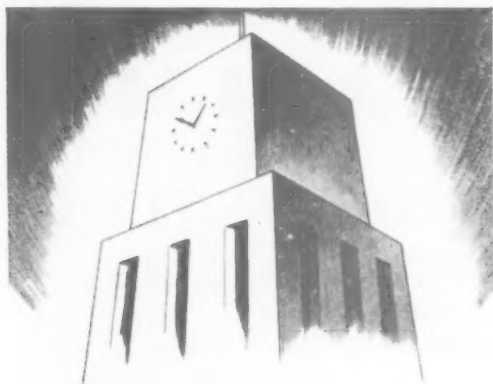
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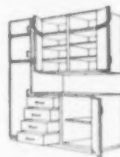
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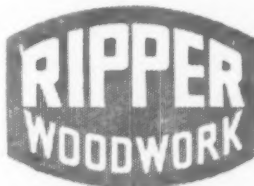


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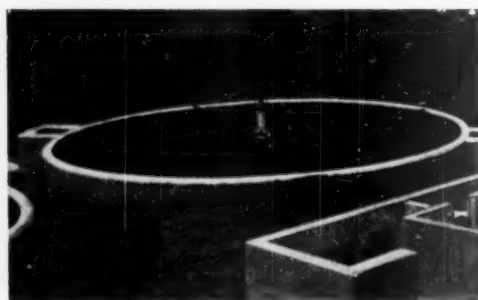
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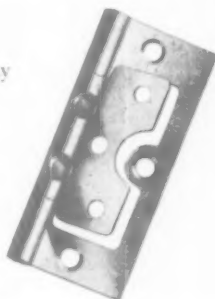
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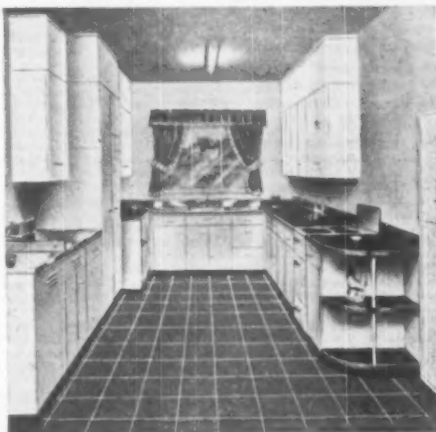


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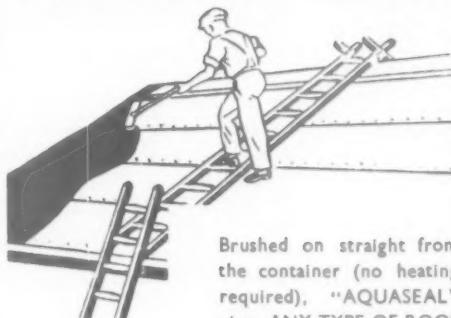
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(c) 2 JUNIOR ASSISTANT ARCHITECTS—A.P.T. Division, Grade III. Salary £450-£495 per annum.

Preference will be given to applicants who have passed the Intermediate R.I.B.A. Examination. Applicants must be capable of preparing working drawings and details for modern buildings.

(d) 1 SPECIFICATION WRITER—A.P.T. Division, Grade IV. Salary £480-£525 per annum.

Applicants for post (d) must have had Architectural and Quantity Surveying experience and possess a thorough knowledge of current Building Practice and Materials. They must be fully capable of preparing detailed Specifications for Building and Painting Works of all descriptions in connection with County Council properties, and be able to prepare inquiries and deal with Provisional Sum Items in connection with new building projects. The successful applicants will also be required to assist generally in the Quantity Surveyor's Department.

The appointments will be subject to termination of one month's notice in writing on either side, and also to the provisions of Local Government Superannuation Act, 1937. The successful applicants will be required to pass a medical examination and to reside in such place in the County as the County Council may direct.

Applications, giving age, qualifications and particulars of present and previous appointments, and accompanied by copies of three recent testimonials, to be sent to me, the undersigned, in a sealed envelope, endorsed with a description of the post applied for, by not later than the 31st day of August, 1950.

W. E. BUFTON, Clerk of the County Council.  
County Offices, Ruthin.  
4th August, 1950. 14746

## MINISTRY OF WORKS.

THERE are vacancies in the Chief Architect's Division for ARCHITECTURAL ASSISTANTS and LEADING ARCHITECTURAL ASSISTANTS with recognised training and fair experience. Successful candidates will be employed in London and elsewhere on a wide variety of Public Buildings, including Atomic Energy and other Research Establishments, Telephone Exchanges, and Housing.

Salary Architectural Assistants £100-£525 per annum. Leading Architectural Assistants £500-£625 per annum. Starting pay will be assessed according to age, qualifications and experience. These rates are for London; a small deduction is made in the Provinces.

Although these are not established posts, some of them have long term possibilities and competitions are held periodically to fill established vacancies.

Apply in writing, stating age, nationality, full details of experience, and locality preferred, to Chief Architect, W.G.10/BC, Ministry of Works, Abell House, London, S.W.1, quoting reference W.G.10/BC. 14671

## BOROUGH OF JARROW.

## BOROUGH ENGINEER'S DEPARTMENT.

## APPOINTMENT OF ARCHITECTURAL ASSISTANT, GRADE IV.

APPLICATIONS are invited for the appointment of ARCHITECTURAL ASSISTANT, A.P.T. IV (£480-£525). Applicants must have an approved Architectural qualification and sound experience, particularly in Housing.

The successful applicant will be required to pass a medical examination. Applications in plain envelopes endorsed "Architectural Assistant," stating age, present and previous appointment, and experience, together with the names and addresses of three persons to whom reference as to ability and character can be made, to be received by the undersigned on or before the 26th August, 1950.

Canvassing directly or indirectly will disqualify, and applicants must state whether or not they are related to any member of, or the holder of any senior office, under the Council.

The Council is prepared, if a successful applicant requires it, to let him have the tenancy of one of the Council's houses.

IAN R. SCUTT, Town Clerk.  
Town Hall,  
Jarrow, Co. Durham.  
3rd August, 1950. 14739

## KENT COUNTY COUNCIL.

## BUILDINGS DEPARTMENT.

APPLICATIONS are invited for an appointment in the Buildings Department of an ARCHITECTURAL ASSISTANT at a salary within the range A.P.T. Grade III/III (£420-£495).

Candidates must have passed the Intermediate Examination of the Royal Institute of British Architects and have had some experience in the preparation of working drawings and development of detail drawings.

The commencing grade and salary will be dependent upon the experience of the successful candidate.

The post is superannuable and the successful applicant will be required to pass a medical examination.

Applications, on forms obtainable from the County Architect, Springfield, Maidstone, should be delivered to him within two weeks of the appearance of this advertisement.

W. L. PLATTS, Clerk of the County Council.  
County Hall, Maidstone.  
31st July, 1950. 14729

## THE LANCASHIRE COUNTY COUNCIL.

## ARCHITECT'S DEPARTMENT.

EXPERIENCED and qualified ARCHITECTS with a flair for design are invited to apply for vacant positions, with commencing salaries of £665 rising to £760 per annum, to assist on a school building programme which includes technical colleges.

Application forms obtainable from the County Architect, County Offices, Preston, to be returned by the 16th September, 1950. 14735

## CITY OF COVENTRY EDUCATION COMMITTEE.

## CLERK OF WORKS.

APPLICATIONS are invited for the appointment of a CLERK OF WORKS on the temporary staff of the Education Department. Maximum salary £12 per week, subject to qualifications and experience.

The appointment is in connection with the erection of one or more schools in the City of Coventry and applicants should have extensive experience of all branches of the building trade, and be capable of setting out, measuring up, keeping records and making reports. The successful applicant will be expected to belong to an appropriate organisation in accordance with Paragraph 44 of the N.J.C. Charter. The appointment will be for about 12 months in the first place.

Applications, on the official form, which can be obtained from the Director of Education, Council House, Coventry, must be returned to him by the 2nd September, endorsed "Temporary Clerk of Works." The appointment is subject to the Standing Orders of the Council and canvassing in any form is prohibited.

W. L. CHINN, Director of Education.  
The Council House, Coventry.  
10th August, 1950. 14752

## CARSHALTON URBAN DISTRICT COUNCIL.

## ARCHITECTURAL ASSISTANT.

APPLICATIONS are invited for the appointment of ARCHITECTURAL ASSISTANT in the Engineer and Surveyor's Department, at a salary in accordance with Grade A.P.T.VI of the National Scale (£595-£660) plus London "Weighting."

Applicants must hold the Final Examination Certificate of the R.I.B.A. and be Registered Architects. They must be competent in design and construction and have had sound experience in the architectural work of a Local Authority.

The appointment will be subject to (1) the provisions of the National Scheme of Conditions of Service, (2) the passing of a medical examination for superannuation purposes, and (3) one month's notice on either side.

The Council cannot provide the successful applicant with housing accommodation.

Applications, on forms obtainable from the undersigned, must be returned, together with the names of three referees, not later than 28th August, 1950.

Canvassing in any form will be a disqualification.

J. W. WRIGHT, Clerk of the Council.  
District Council Offices,  
The Grove, Carshalton. 14718

BRACKNELL DEVELOPMENT CORPORATION (Bracknell, Berks) invites applications from suitably qualified persons for the following appointments—

(1) PLANNER. Salary £750 x £50 - £900.  
(2) ARCHITECT (Housing). Salary £550 x £40 - £750.

Applicants for (1) should be Corporate Members of the T.P.I. and also have a sound knowledge of town planning, with particular reference to the problems of new town development.

Applicants for (2) should be Corporate Members of the R.I.B.A. and an additional town planning qualification will be an advantage. They should have had experience of the design of house types and the layout and construction of large housing developments.

The successful applicants will work under the direction of E. A. Ferry, B.Arch., A.R.I.B.A., A.M.T.P.I., Chief Architect to the Corporation.

The posts will be superannuable under the Local Government Superannuation Act, 1937, and the successful candidate will be required to pass a medical examination.

Applications, giving full particulars of the candidate's age, qualifications and experience, together with the names of two persons to whom reference can be made, must reach the General Manager, Bracknell Development Corporation, Farley Hall, Binfield, Bracknell, Berks. on or before 31st August, 1950. 14749

AYCLIFFE DEVELOPMENT CORPORATION  
(Established under the New Towns Act, 1946).

## APPOINTMENT OF ASSISTANT ARCHITECTS.

THE above Corporation invite applications from ARCHITECTS for appointment on the staff of their Chief Architect, Mr. G. A. Goldstraw, B.A., A.R.I.B.A., at salaries in accordance with Grades A.P.T. V and A.P.T. VII of the National Joint Council for Local Authorities, Administrative, Professional and Technical Services, i.e., £520 rising to £570, and £615 rising to £710 respectively. Where necessary, housing accommodation will be available.

Applications, stating qualifications, previous experience, present appointment, salary, age, and the Grade applied for, must be forwarded to the undersigned not later than 6th August, 1950.

The applicant should include the names and addresses of three persons to whom personal reference can be made if necessary.

(Signed) A. W. THOMAS, General Manager.  
Newton Aycliffe, Co. Durham.  
2nd August, 1950. 14731

## LONDON COUNTY COUNCIL.

## ARCHITECT'S DEPARTMENT.

APPLICATIONS are invited for positions of ARCHITECT, Grade III (£550-£700) and TECHNICAL ASSISTANT (up to £580) for work on new housing schools and other public buildings. The positions are superannuable. Candidates for Grade III positions should possess professional qualifications—Applications on forms from the Architect (A.P./P.S.), The County Hall, Westminster Bridge, S.E.1, enclosing stamped addressed foolscap envelope. Canvassing disqualifies (384). 10097

**EAST RIDING OF YORKSHIRE COUNTY COUNCIL.****COUNTY ARCHITECT'S DEPARTMENT  
CLERKS OF WORKS.**

APPLICATIONS are invited for the appointments of RESIDENT CLERKS OF WORKS for the Rosebery Avenue County Primary School at Bridlington and for the Market Weighton County Secondary School.

The salaries will be up to £9 15s. 0d. per week in accordance with qualifications.

The persons appointed will be required to take charge of the contract and must possess appropriate qualifications and experience.

The appointments are terminable by one month's notice on either side. The successful candidates will be required to pass a medical examination. Applications, stating age, qualifications and experience, with particulars of past and present employment and salaries, together with copies of three recent testimonials, must be received by the County Architect, County Hall, Beverley, not later than Friday, 25th August, 1950. Applicants should disclose relationship to any member or senior officer of the Council and canvassing will be a disqualification.

T. STEPHENSON, Clerk of the Council.  
County Hall, Beverley.  
August, 1950. [4750]

**AIR MINISTRY** have vacancies for DESIGNERS/DRAUGHTSMEN in the Design Branch of the Works Department for high class work in the following fields: Architecture, Drainage and Water Supply, Land Survey. The work includes designs for London Airport. Salaries are on ranges up to £750. Starting pay according to age and qualifications. Applications, stating age, qualifications, previous appointments and salary required should be sent to Air Ministry, S 200, Cornwall House, London, S.E.1. It is regretted that applications of candidates not called for interview cannot be acknowledged. [4753]

**LONDON COUNTY COUNCIL.**

APPLICATIONS are invited for positions of ARCHITECTURAL ASSISTANT (salaries up to £580 a year) in the Housing and Valuation Department. Commencing salaries will be determined according to qualifications and experience. Engagement will be subject to the Local Government Superannuation Acts, and successful candidates will be eligible for consideration for appointment to the permanent staff on the occurrence of vacancies.

Successful candidates will be required to assist in the design, layout and preparation of working drawings for housing schemes (cottages and multi-storey flats) and will be employed in the Housing Architect's Division.

Forms of application may be obtained from the Director of Housing, The County Hall, Westminster Bridge, S.E.1 (stamped addressed envelope required and quote reference A.A.1). CANVASSING disqualified. (816). [0101]

**CONTRACTS****COUNTY BOROUGH OF BRIGHTON.****TO: BUILDERS AND CONTRACTORS.**

TENDERS are invited for the ERECTION of 26 HOUSES (Section 463), Lower Beveand Estate.

Bills of Quantities will be available on or after Saturday, 12th August, 1950.

Builders and Contractors desiring to tender should apply to the Borough Engineer and Surveyor, Mr. D. J. Howe, M.I.C.E., M.I.Mun.E., 25-30 King's Road, Brighton, and enclose a deposit of £2 2s. 0d. (made payable to the Brighton Corporation) which will be refunded on receipt of a bona-fide tender or notification of inability to tender, providing such is received before the latest date of tendering.

The Plans may be viewed during the usual office hours.

Sealed tenders, suitably endorsed, are to be delivered in the enclosed envelope provided to the undersigned not later than 12 noon Monday, the 4th September, 1950.

The successful tenderer will be required to enter into a Bond with an approved Guarantee Insurance Company or Bank for the due and proper completion of the work.

The Council do not bind themselves to accept the lowest or any tender.

J. G. DREW, Town Clerk.  
Town Hall, Brighton.  
3rd August, 1950. [4747]

**COUNTY BOROUGH OF BRIGHTON.****TO: BUILDERS AND CONTRACTORS.**

TENDERS are invited for the ERECTION OF HOUSES on the Coldean Estate, Parkside Extension, as follows:—

20 Houses	Section 2D
18 Houses	Section 2E
29 Houses	Section 3A
30 Houses	Section 3B

Bills of Quantities will be available on or after Saturday, 12th August, 1950.

Builders and Contractors desiring to tender should apply to the Borough Engineer and Surveyor, Mr. D. J. Howe, M.I.C.E., M.I.Mun.E., 25-30 King's Road, Brighton, and enclose a deposit of £2 2s. 0d. for each section (made payable to the Brighton Corporation) which will be refunded on receipt of a bona-fide tender or notification of inability to tender, providing such is received before the latest date of tendering.

The Plans may be viewed during the usual office hours.

Sealed tenders, suitably endorsed, are to be delivered in the enclosed envelope provided to the undersigned not later than 12 noon Monday, the 4th September, 1950.

The successful tenderer will be required to enter into a Bond with an approved Guarantee Insurance Company or Bank for the due and proper completion of the work.

The Council do not bind themselves to accept the lowest or any tender.

J. G. DREW, Town Clerk.  
Town Hall, Brighton.  
3rd August, 1950. [4748]

**METROPOLITAN BOROUGH OF DEPTFORD.****ERECTION OF A FOUR-STOREY BLOCK OF  
TWENTY FLATS.  
MALPAS ROAD (WEST) SITE.**

THE Council invites applications from Contractors desirous of submitting tenders for the erection of a block of twenty Traditional-Type Flats in four storeys on the Malpas Road (West) Site, S.E.4.

Contractors wishing to apply for permission to tender should inform the undersigned not later than Saturday, 26th August, 1950, at the same time giving details of similar work which they have recently executed.

Selected tenders will be notified in due course, and will be issued with Bills of Quantities, etc., on receipt of a deposit of two guineas, which will be returned upon submission of a bona-fide tender, which is not subsequently withdrawn.

Plans may be inspected at the offices of the Council's Architects, Messrs. H. V. Ashley & Winton Newman, 3 Verulam Buildings, Gray's Inn, W.C.1.

The Council does not bind itself to accept the lowest or any tender or incur any cost in connection with the preparation of any tender.

ERNEST FIELD, Town Clerk.  
Deptford Town Hall  
New Cross Road, S.E.14.  
10th August, 1950. [4751]

**COUNTY OF LINCOLN—  
PARTS OF LINDSEY.****PROPOSED SUPERINTENDENT'S HOUSE  
AND TWO INSPECTOR'S HOUSES.  
TOWN HALL SQUARE, SCUNTHORPE.**

CONTRACTORS desirous of tendering for the above Houses should submit their names to the Architect, Messrs. Charles B. Pearson & Son, F.R.I.B.A., 18 Dilton Square, Lancaster, not later than Wednesday, 30th August, 1950.

Bills of Quantities and Specification will be sent to intending tenders. Plans may be seen at any office during normal working hours, or at the offices of the Architect, or the Clerk of Works' Office on the site.

The acceptance of any tender is subject to the approval of the Home Office and the Council do not bind themselves to accept the lowest or any tender.

HERBERT COPLAND, Clerk of the Council.  
County Offices, Lincoln.  
2nd August, 1950. [4750]

**THE URBAN DISTRICT COUNCIL OF  
ABERDARE.****TO: BUILDING CONTRACTORS.****CONSTRUCTION OF NINE SHOPS, SIX FLATS,  
GARAGES AND PUBLIC CONVENIENCE.**

THE Urban District Council invites tenders from suitable building contractors for the construction of nine shops, six flats, six garages and a public convenience, together with appurtenant works on a site situated at Penywaun Neighbourhood, Aberdare.

Plans, Specification, Bill of Quantities and Form of Tender may be obtained from the Office of the Engineer and Surveyor, Vestry Hall, Aberdare, on receipt of a fee of two guineas, which will be returned in exchange for a bona-fide tender together with all loaned documents.

The Council do not bind itself to accept the lowest or any tender.

E. D. REES, Clerk of the Council.  
Town Hall, Aberdare, Glam.  
14th August, 1950. [4754]

**ARCHITECTURAL APPOINTMENTS  
VACANT**

APPLICATIONS are invited for positions as ASSISTANT ARCHITECTS in an Architect's Office of the Civil Engineer's Department, British Railways, located in London. Assistants will be engaged on large Station Reconstruction Schemes and should be A.R.I.B.A. or hold an equivalent qualification. The salary offered is up to £550 per annum dependent on qualification and experience. The posts are temporary.—Apply, stating age, qualifications and experience to Box 5774, The Architect and Building News. [4722]

ARCHITECTURAL Assistant required for work in Central and Greater London. Final R.I.B.A. standard desirable. High salary according to ability and experience. Five-day week.—Apply Box 5893, The Architect and Building News. [4744]

**SITUATIONS VACANT**

ARCHITECTURAL Metal Workers require a Designer-Draughtsman of considerable merit. Top salaried position for skilled man.—Apply The Morris Singer Company, Hope House, St. Peter Street, Westminster, S.W.1. [0095]

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